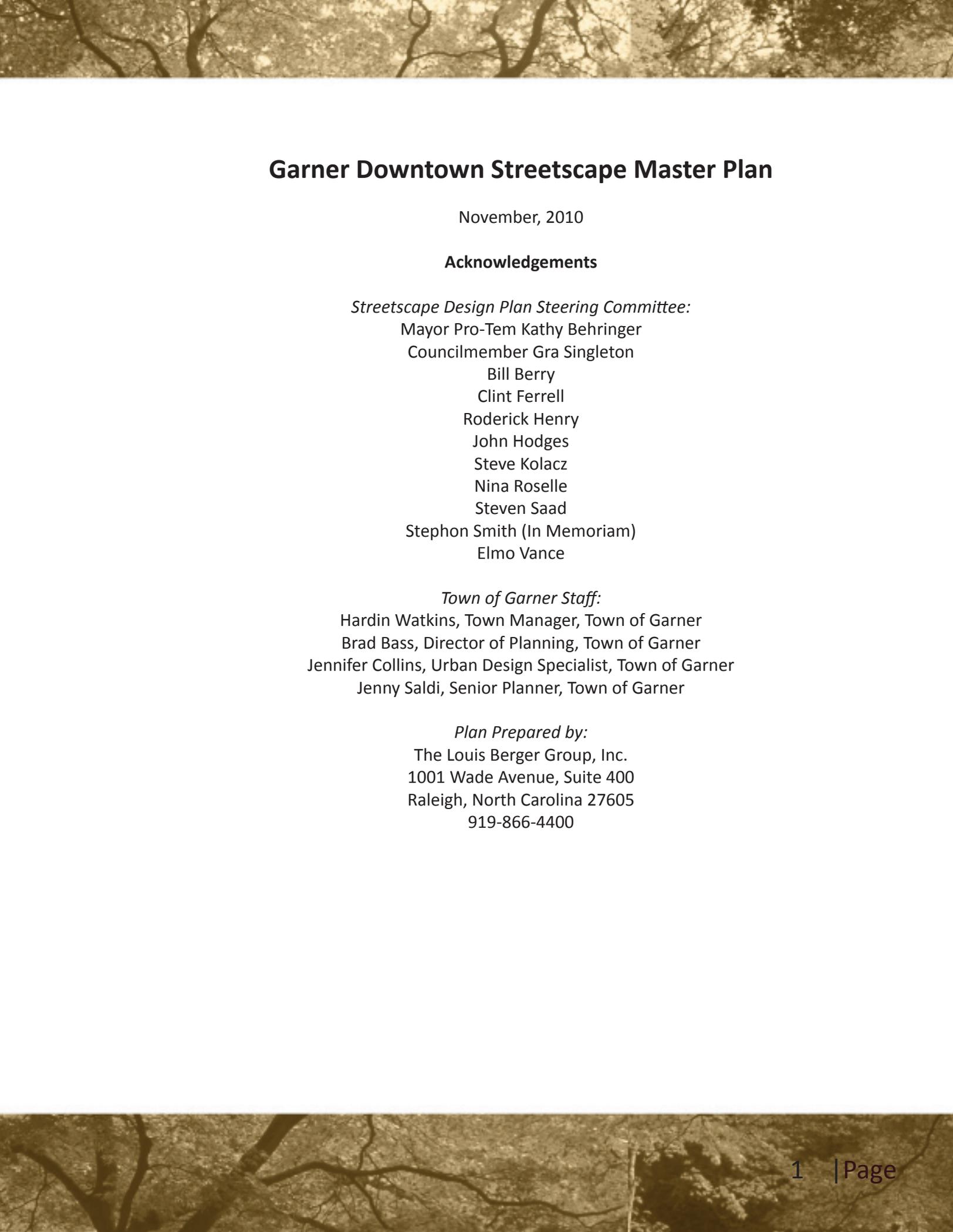


Town of Garner

Streetscape Master Plan





Garner Downtown Streetscape Master Plan

November, 2010

Acknowledgements

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History

TRANSPORTATION HAS PLAYED A MAJOR ROLE in defining Garner's history, first with the expansion of the railroad through the area in 1847; then with Garner's position along North Carolina's Central Highway, which linked the mountains to the sea in the early 1900s. Today, transportation has emerged as a focal point again for Garner, only this time it is centered on commuter rail and new transportation corridors that link Triangle communities. The Central Highway has become US 70, which now bypasses downtown Garner, but still provides a vital link in the town's and region's transportation system. The Southern Railroad is now owned by the State of North Carolina and is part of a strategic plan to connect the Triangle region and other reaches of the state with passenger and commuter rail.

It would have been hard to envision a present-day Garner with more than 26,000 residents when the area was originally incorporated in 1883 under the name Garner's Station. The name itself is of unknown origins as the original postmaster, Thomas Bingham, had requested the name Garner's Station in 1878 from the postal service and then departed shortly thereafter to Clayton without ever explaining the name.

Furthering the Town’s mysterious origins, the Village of Garner’s Station had its charter temporarily revoked beginning in 1891. Its limits covered only 400 square yards at that time and had grown to only a half-square mile when the area was re-incorporated in 1905 as the Town of Garner.

The Town’s early days reflect a common historical link to other municipalities and their issues at the time, as well as current issues of today. The Town began paying its Mayor a stipend of \$125 per year in 1907 after an expansion of local authority, and also began taxing the owners of dogs that would run loose on the streets. License fees for certain merchants – farmers were almost universally excepted – were commonplace and incredibly detailed: rope / wire dancing, tumbling, traveling salesmen, itinerant photographers were all subject to a business license. By the mid-1910’s a portion of Garner Road had been paved with concrete to allay safety concerns, the first paved road in the State and predating electricity in the Town by nearly a decade. A five-mile stretch between Garner and Raleigh was delayed to work out right-of-way issues with Southern Railway.

By 1950, the passenger rail era had passed and the town’s population had reached only 1,200 residents. This modest population growth is attributable to the agrarian lifestyle that dominated Garner for almost 200 years, from the founding days of Wake County in the 1770s to the 1950s when cotton and tobacco were the primary crops grown in the area. As state government grew and Raleigh expanded after the 1950s, existing Garner residents and newcomers were drawn to government, education and other jobs in North Carolina’s Capital City, the Research Triangle and nearby communities. While the agrarian era has passed, Garner has significant remnants of agricultural and industrial uses within the town limits. The 2006 Comprehensive Growth Plan indicated approximately 4,500 acres of agricultural land, primarily east of Benson Road.

Much of Garner’s recent growth, however, has been reflective of its position adjacent to Raleigh and other municipalities, colleges, universities, employment centers, museums and seasonal events like the State Fair. The small-town character combined with proximity to regional job centers, Interstate 40, other key regional corridors and the hub of state government has created an urban and suburban society that is highly dependent on the automobile for daily travel.

The Design Process

IN EVERY SUCCESSFUL DESIGN PROCESS clear steps must be outlined along with achievable benchmarks that can move a community from cloud of ambiguity to the clarity of a community based master plan.

Downtown Garner 1918



Downtown Garner Today

The planning study presented in the following sections focuses on Garner’s downtown core area as well as its surrounds. The resulting streetscape master plan provides a vision and framework for future development within this core area as well as the community as a whole. Unique to this study was the Town of Garner’s emphasis on public involvement and participation with the plan being developed by the community itself through a steering committee and public meetings as well as project web site for dissemination and gathering of information.

The steering committee was appointed by Garner’s City Council and played the key role in developing the streetscape master plan. The steering committee acted as a bridge to community as was composed of a diverse group of citizens, business owners, council members, and professionals alike. Through facilitated workshops this committee conceived, developed, and finalized the plan. Additionally, several public workshops were facilitated during the process in order to gain further input and understanding of the study area.

Process Overview

- Guiding Principals
- Identify Project Limits and Focus Areas
- Review of Existing Conditions
- Develop a Project Vision
- Develop Project Goals
- Challenges and Opportunities
- Conceptual Development
- Conduct Public Workshop
- Development of Conceptual Alternatives
- Development of the Preferred Alternative
- Conduct Public Workshop
- Preliminary Master Plan Document
- Final Master Plan Document

Garner Streetscape Design 14 Jan 09
Uplifting, Restoring, and Evolving
Downtown Garner

What design considerations should be part of Garner’s Downtown Streetscape Plan

- Connectivity
- Being ‘Green’
- Utilities
- The Visual Message
- Financing
- Revitalization



Connectivity	Being “Green”	Utilities	Visual Message	Financing	Revitalization
<ul style="list-style-type: none"> • Connect and complete sidewalks • Link to Greenways • Public Transportation • Family and ADA Friendly • Bus Friendly 	<ul style="list-style-type: none"> • More Green Space • Sustainable • Trash and Recycling Receptacles • LEED Criteria 	<ul style="list-style-type: none"> • Underground Utilities • Storm Drainage Planning • Enhanced Historic Lighting • Stormwater management 	<ul style="list-style-type: none"> • Landscaping that reflects individual location • Appropriate Zoning for Downtown • Hwy 50 Railway Bridge-improve/replace • Impactful Gateway-monument/signage / sidewalks • Visually Appealing • Historic Downtown Focus • Train Station Museum reflected in streetscape • Seasonal Banners 	<ul style="list-style-type: none"> • Who will pay for improvements • Impact fee credits for development 	<ul style="list-style-type: none"> • Improve appearance around Historic Auditorium • Eliminate blight

Visioning

An important objective of the study was involvement of the steering committee which, included: neighborhood representatives, government officials, government staff, developers, and business owners. The outcome of the workshop was a clearly defined vision. following problem statement was presented during the Visioning Session: **What design considerations should be part of Garner's Downtown Streetscape Plan.** The group then divided into pairs and began brainstorming and writing down ideas that applied the problem statement. The ideas were then categorized then prioritized. The categories are as follows:

- Connectivity
- Being 'Green'
- Utilities
- The Visual Message
- Financing
- Revitalization

Opportunities and Challenges

Following the Visioning Session an Opportunities and Challenges Session in order bring to light all aspects of the design of a streetscape for Downtown Garner. Opportunities and Challenges were further divided into Design and Planning. The results are presented on the following page.



Design Opportunities Cards

- Develop Landscape and Hardscape Standards
- Link the Community together
- A Place for Programmed Events
- Brand Downtown
- Downtown has Three Current Destinations
- Improve access
- Tie to way finding-NCDOT
- Improve interpretation
- Bike Connections
- Parks and Greenspace
- Openspace connect with Downtown
- Expand Development
- Local History
- Signage: Historic/ Interpretive

Planning Opportunities Cards

- Stimulus Package
- Encourage Business and Consumer Investment
- Tie the Community Together
- Programmed Events
- New Governor
- Activate Community
- Nearby Advocate-Ross
- Connection with Broader Tourism
- Raise Awareness of Town Involvement
- Become a model for other Communities
- Increase Property Value
- Leverage Private Investment
- Change Negative Image
- Live-Work-Play
- Green approach
- Small victories-project priorities
- Harness Private Investment
- Streetscape Should Be “Better than Good Enough”

Design Challenges Cards

- Railroad
- Linkages and Gateways
- Downtown Identity
- Our Message
- NCDOT
- Diversity of Landscape

Planning Challenges Cards

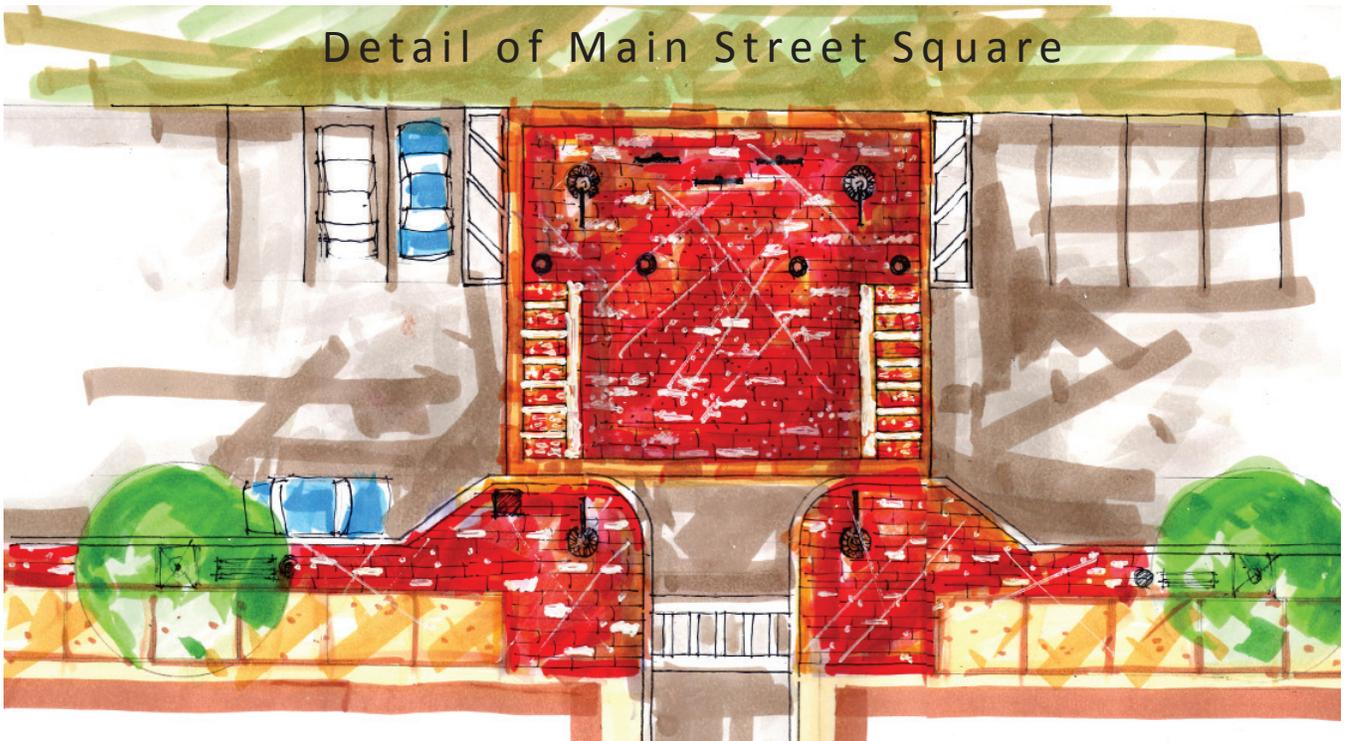
- Funding
- Budgeting
- Community Opinion
- Balance with Town Priorities
- Getting and Retaining Businesses
- Community “Buy-In”
- Private Development
- Rental Properties
- Getting Our Message Out
- Mediating Business Interruption
- Rental Properties
- Getting beyond “We Can’t”
- Stigma of Stagnation
- Poor Economic Timing

Conceptual Workshop

THE CONCEPTUAL WORKSHOP IS THE BRIDGE BETWEEN VISIONING AND THE FINAL PLAN of Garner's Downtown Streetscape. This is where ideas begin to take physical form. This transitional phase sought to refine the Visioning Categories of Connectivity, Being 'Green, Utilities, The Visual Message, Financing, and Revitalization. These categories were then grouped into three themes: *Historic, Green, and Linkages*. The Steering Committee was then divided into three groups, one per each theme. The groupings were based upon each committee member's interest and expertise. As a starting point *Design Considerations* of each Theme were presented. These considerations were derived from the Visioning workshop and served as a point of discussion within the groups. Each group was facilitated by a member of Berger's or Garner's Staff who assisted with base maps to develop a master plan from each particular focus theme. After the plans were developed, each group presented its concept to the steering committee followed by questions and answers. Programmatic items of each theme were documented. Items that were not common to all themes were then listed and discussed to determine their inclusion within the Composite Master Plan. All agreed-upon items were listed and represent the Composite Master Plan Program.

Conceptual Design Workshop





Composite Master Plan

THE PROGRAMMATIC AND DESIGN ELEMENTS of Garner's Downtown Streetscape as taken from the workshop are as follows:

- Sidewalks along Main Street
- Could incorporate logo along sidewalk to denote Historic Trail
- Cobble stone or brick surface along sidewalks on Main Street
- Remove cobra lights along Main Street
- Move existing lamp post along Residential Portion of Main Street
- Add lamp post that can accommodate banners along commercial section of Main Street-lights must compliment existing features
- Add two lamp post with banners at the intersections of Main and Benson and Main and New Rand Roads in conjunction with "Friends of Garner" gateway signs
- Furnishings along Main Street including: benches, bike racks, trash receptacles (could be developed by local artisan)
- Landscape groupings along Garner Road Corridor
- Side walks from Senior Center to Jones Sausage
- Signage program for historic buildings and significant features (bronze signage)
- Recycling facilities at existing park
- Multi-use trail with trail heads at park facilities with parking and bike racks
- Pedestrian crossings along St Marys at Garner Road and Main Street
- Pedestrian crossings along New Rand Road at Garner Road and Main Street
- Link north and south Garner across US 70 via multi-use trail along Benson Road-tie to future greenspace on south side
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Add pedestrian crossing at intersections
- Develop planting scheme between Benson and New Rand Roads along the island on US 70
- Sidewalk/ trail enhancements for walking and biking on semi-impervious/ porous paving-should be considered
- Expand bus route from US 70 to hwy 50 Benson Road to Garner Road to New Rand Road back to hwy 70 to White Oak
- Add transit stop at Senior Center
- Develop bike routes connecting residences to business and parks
- Add bike lanes along Garner Road
- Develop transit gateway at New Rand Road and US 70 to provide crossing from north to south ends of town for pedestrians and bikes
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Develop decorative directional signs
- Add bike lanes to connect to greenways
- Connect parks with sidewalks
- Add bike lane along Benson Road include bike lanes on bridge replacement
- Light Rail/ Commuter Rail Stations located between Saint Marys Street and Benson Road-should be considered
- Green lighting(LED)- should be considered
- Public Art should inform the street furniture
- Black wrought iron theme for furnishings throughout plan
- Pavers on Main Street pavers over buried utilities-show alternative materials and cost comparisons
- Stormwater problem areas-clean water trust fund money
- Appropriate plantings along each unique area
- Need parking study for main street
- Sharrows if bike lanes are not possible
- Transportation plan needs to determine bike lanes or multi-use trail
- Transportation plan needs to determine if there should be roundabouts at intersections of New Rand Road and Garner Road and Benson and Garner Road
- Move depot and caboose to Garner Recreational Park

Study Definition

THE TOWN OF GARNER is located approximately eight miles south of downtown Raleigh, and about 15 miles southeast of Research Triangle Park and Raleigh-Durham International Airport. The

The goal of the Streetscape Design is to physically articulate the uniqueness of Garner's community, its history, its people and future growth and development. The design address areas of concern such as road cross-section needs,



Brad Bass, Director of Planning, at Conceptual Workshop

proximity to these major employers, as well as being proximate to important watersheds and the high growth areas of Johnston County and eastern Wake County, has shaped the development of the Town. This strategic position offers great incentives for smart growth, investment, transit-oriented development, redevelopment, and revitalization. In turn, the Streetscape Design for Garner Road and Main Street has the potential to be an economic catalyst for all of Downtown Garner.

pedestrian facility needs, safety issues, street lighting needs, landscape needs, sight furnishing needs, parking, utility issues and all other considerations.

The Streetscape Design, in combination with the Transportation Plan, has the opportunity to be the beginnings of a map for making important land use, infrastructure, and public service decisions for all of Garner. Our approach to the streetscape master plan ensured that all aspects of planning and design were considered. Our team guided The

Town of Garner through an engaging design process that identified opportunities, addressed issues, established a strong achievable Vision and developed priorities that will lead to successful implementation. In short, our approach was a flexible process with the ability to adjust to the project needs throughout.

A key component of our process was the establishment of a core group that would help shape and steer the design process. This group was comprised of citizens from Garner itself, from business people, to homeowners, retirees, local and state employees, individuals with planning backgrounds, and wide range of cultural diversity. This group was appointed by the Town Council and became the steering committee that shaped the streetscape plan as described in the follow-

ing pages. In addition to the steering committee a strong effort was made to connect with the community at large to gain input as well as providing a system of checks and balances to ensure the plan was developing in a direction the Town of Garner was supportive of. As a part of the outreach information about the project was distributed through several mailings as well as input

gathered in public meetings. Additionally, a project web site was established and maintained where the citizenry could check on the progress of the plan as well as complete surveys and provide ad-

ditional input. Through the efforts of town leaders, the steering committee, and the community at large, we have developed a plan that represents the unique character of Garner while addressing current and future needs.



Streetscape Project Web Site



History

TRANSPORTATION HAS PLAYED A MAJOR ROLE in defining Garner's history, first with the expansion of the railroad through the area in 1847; then with Garner's position along North Carolina's Central Highway, which linked the mountains to the sea in the early 1900s. Today, transportation has emerged as a focal point again for Garner, only this time it is centered on commuter rail and new transportation corridors that link Triangle communities. The Central Highway has become US 70, which now bypasses downtown Garner, but still provides a vital link in the town's and region's transportation system. The Southern Railroad is now owned by the State of North Carolina and is part of a strategic plan to connect the Triangle region and other reaches of the state with passenger and commuter rail.

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View of Main Street looking towards New Rand Road



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nearby communities. While the agrarian era has passed, Garner has significant remnants of agricultural and industrial uses within the town limits. The 2006 Comprehensive Growth Plan indicated approximately 4,500 acres of agricultural land, primarily east of Benson Road.

Much of Garner's recent growth, however, has been reflective of its position adjacent to Raleigh and other municipalities, colleges, universities, employment centers, museums and seasonal events like the State Fair. The small-town character combined with proximity to regional job centers, Interstate 40, other key regional corridors and the hub of state government has created an urban and suburban society that is highly dependent on the automobile for daily travel.

Garner's strategic location along the North Carolina Railroad has given the Town an opportunity to re-create its history as a community depen-

dent on the rails. In the early 1900s, one could hop the train to Raleigh for a dime in the morning or afternoon and make the return by noon or 8 o'clock in the evening. Today, it is envisioned that peak period commuter rail services could once again connect downtown Garner to Raleigh, Durham and Greensboro as well as points east to Goldsboro.

The footprint of Garner's history – its downtown - remains largely intact from its early days. The narrow strip of commercial buildings fronting the railroad tracks and Main Street; the old neighborhoods south of downtown are part of what created the small-town feel; and the larger estate homes along Garner Road (formerly the Central Highway, and the first paved road in North Carolina) reflect a bygone era when well-to-do citizens desired a highly visible location for their residences along new fashionable highways.

Like many North Carolina locales, Garner's downtown has yet to fully recover from the decline in railroad traffic and the impacts of bypassing the central business district. As the town and region grew, the traffic volumes for Garner Road (then US 70) became so burdensome that the State sought a route to relieve downtown Garner from traffic congestion. The current alignment of US 70 is now a congested roadway that will likely require further relief in the future.

Meanwhile, the lack of traffic volumes in the tens of thousands per day along Main Street and Garner Road may be seen as an advantage for the Town of Garner and the future of its downtown. The Town's residents now desire a historic and cultural centerpiece for their



community and a renewed sense of place for downtown. Downtown Garner offers that opportunity and the Downtown Streetscape Plan will identify the design treatments and strategies to create this centerpiece.



Beyond downtown, Garner has grown into a collection of commercial and residential developments, each reflective of its era in terms of design and orientation. The Cloverdale, Hilltop, Forest Hills and Heather Hills housing developments were the first of their kind in Garner and are indicative of 1950s and 1960s era ranch-style homes. The growth patterns since then have primarily followed nationwide trends in modern housing and commercial development practices, with a division of uses and lack of connectivity within the roadway system.

The Town's roadway system is also a collection of streets that reflect the era in which they were constructed. Meandering country roads traverse rolling hills in and around Garner and many previously served as farm-to-market corridors. While Garner's recent growth has strained the transportation system to the point that capacity improvements are desired along many of Garner's key corridors, many of these corridors remain unimproved from their original dimensions. When combined with the desires of current residents for more of a sense of place and aesthetics, there are many positive examples within Garner to build upon in developing a new Transportation Plan to define how and where Garner's current and future residents will go. Clearly, Garner's history as a crossroads of transportation will also help define its future.

Recent Studies

SEVERAL PLANNING EFFORTS COMPLETED IN THE PAST 10 YEARS will help define portions of the Town of Garner Transportation Plan and Downtown Streetscape Plan. Below is a summary of those documents and how they will relate to or inform the current planning effort.

North Garner Plan (2004).

The North Garner Plan developed as a small area plan with a primary focus on downtown Garner. The Downtown Streetscape Plan is a direct result of the North Garner Plan, which included several recommendations for the transportation system in the area. These included:

- Enhanced appearance and function of Garner Road;
- Investment in existing neighborhoods;
- Multi-modal system enhancements;
- A bikeway map for the North Garner area; and
- A connection of Rand Mill Road to US 70.

The prospects of the railroad emerging as a commuter rail corridor were also a major component of the plan, which identified potential station areas and opportunities for transit-supportive residential densities within ½-mile of the station. The Plan acknowledged that a commuter bus service could be the short-term option for service to this area, but that it was advantageous for Garner to plan for land uses to support the future commuter rail option.

Comprehensive Growth Plan (2006). The land use plan for Garner was updated in 2006 and provides direction for growth in the town through 2020. The 2006 Plan continued the focus on nodal development previously adopted in the 1989 Plan, including a desire for regional centers and transportation corridors to connect these nodes. Citizen input indicated that there was strong desire to establish a community focal point and create a cultural and historic centerpiece for Garner, as well as a sense of place for the downtown area. The Plan also identified the need for design improvements to new development and public spaces, which included streetscape improvements on the Garner Road corridor, street design guidelines and improved aesthetics for US 70 and US 401. Additional transportation recommendations included: a need for more east-west corridors; improvements to the bus system; greater connectivity of major and minor roadways; and strategic expansion or extension to some corridors.

2025 Thoroughfare Plan (2004).

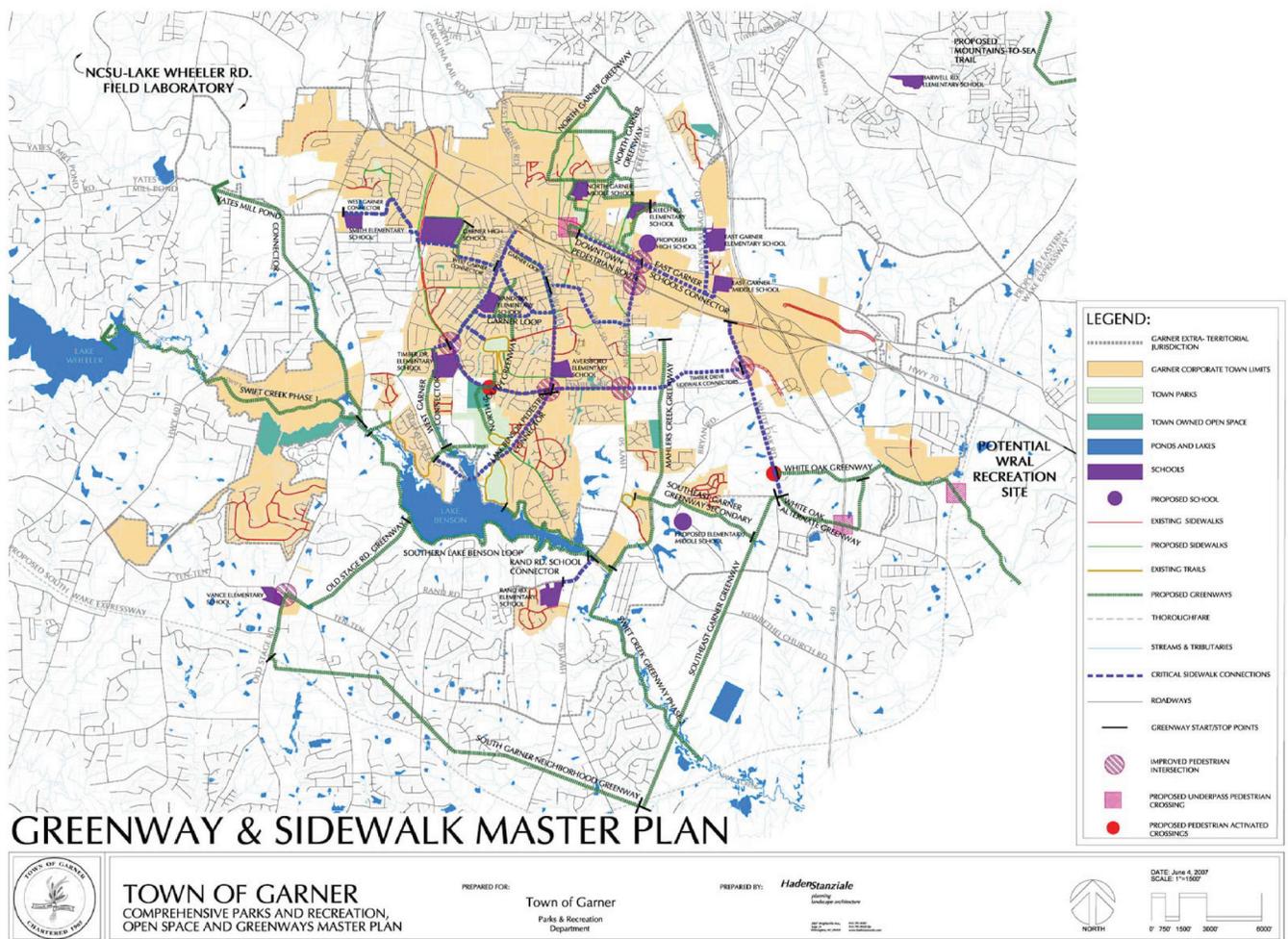
The original 2025 Thoroughfare Plan was developed in 1999 and amended in 2004. The result of the plan is a map indicating existing and proposed freeways as well as major and minor thoroughfares. The facilities identified on the map consist primarily of roadways under the authority of the North Carolina Department of Transportation. Major new facilities identified in the Plan included: the extension of Timber Drive to the east, with a split connect-

ing to Jones Sausage Road and Greenfield Parkway; the extension of Vandora Springs Road to the west connecting to US 401 and Lake Wheeler Road; an interchange at the intersection of US 70 and Timber Drive in northwest Garner; and the Outer Beltway.

Parks and Greenways Master Plan (2007). The Parks and Greenways Master Plan identified several action items to expand and enhance the system of parks and greenways provided by the Town of Garner. Of particular interest are the recommendations for Greenway and Sidewalk Needs contained in Chapter 4 of the 2007 Plan. There was nearly unanimous consensus that a greenway system was needed in Garner to link community gathering places. Several immediate and long-term facilities were identi-

fied as part of this effort and will help guide the Transportation Plan's update of the Sidewalk Master Plan and identify street design components to further these goals. Of particular interest are: the Downtown Pedestrian Route that is proposed to link North Garner with US 70; the six-mile Garner Sidewalk Loop to connect the library, parks, Town complex and schools; and the overall greenway system. Some of the recreation facility needs identified in the plan may also inform the Downtown Streetscape Plan as this design effort may incorporate some of these facilities into the preferred design.

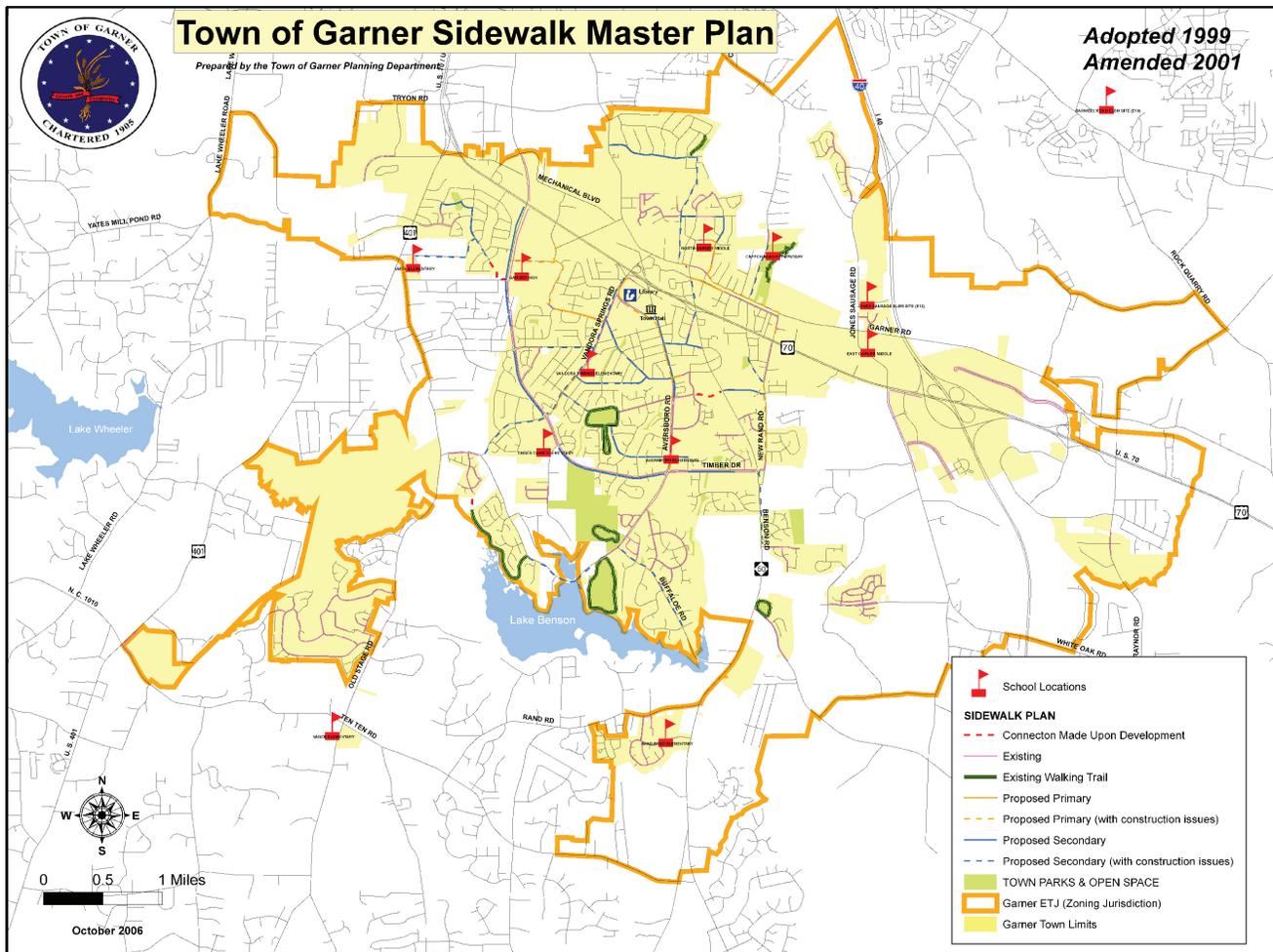
Sidewalk Master Plan (2001). The sidewalk master planning effort resulted in a map covering the Garner Extraterritorial Jurisdiction (ETJ) as of 2001. The plan identified existing



sidewalks and greenways and proposed primary and secondary connections. New facility recommendations consisted solely of improvements connecting the sidewalk system to schools, Town Hall and the library. Although not specifically documented on the map, the plan also identified those improvements that could post a construc-

was limited inclusion of transportation-specific measures in the plan, there were some mitigation actions that related to the Streetscape and Transportation Plan for Garner. These include:

- Keeping infrastructure extensions out of hazardous areas in order limit development in known hazardous areas;



tion constraint. The Sidewalk Master Plan will be updated as part of this Transportation Plan. Hazard Mitigation Plan (2004). The Hazard Mitigation Plan was developed to comply with federal and state requirements and to reduce Garner's exposure to natural hazards by limiting development in hazard sensitive areas, including floodplain or flood hazard areas. While there

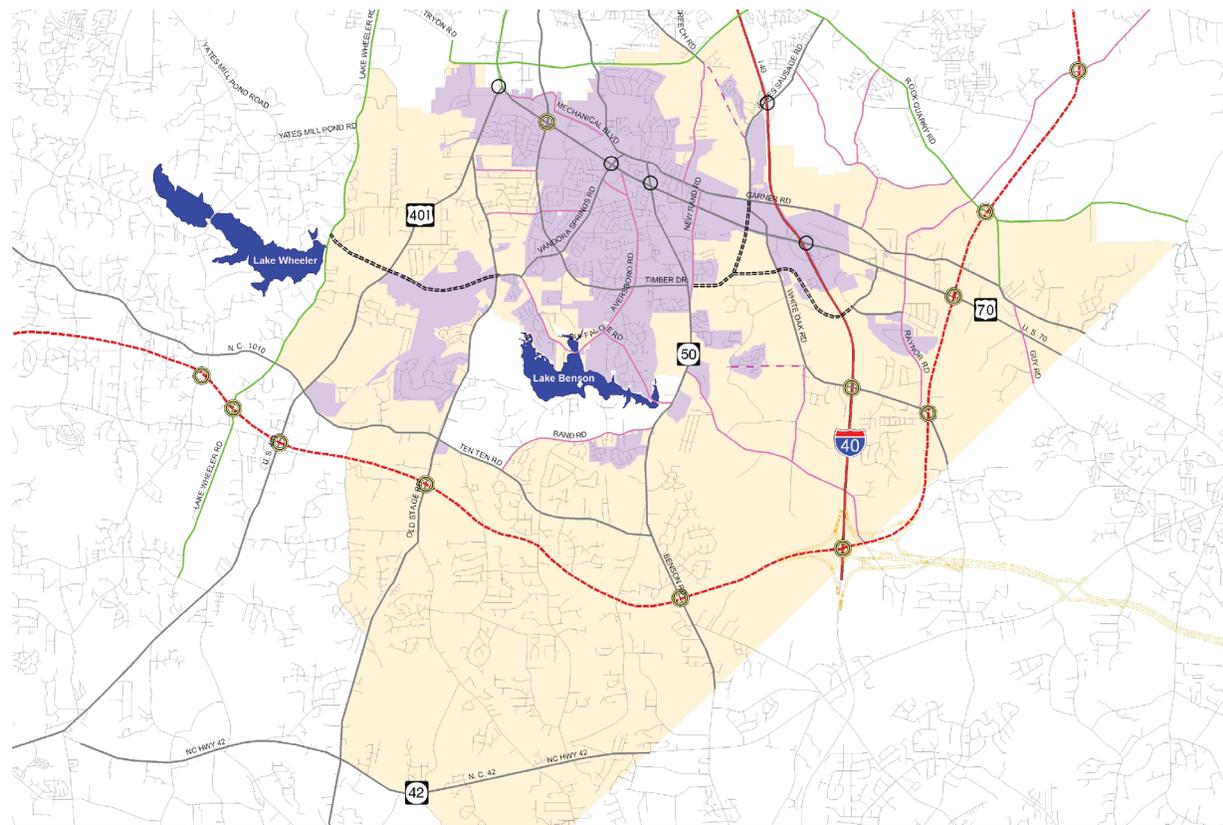
- Zoning ordinances that minimize impervious surface coverage;
- Implementing soil erosion and sedimentation control measures in the development approval process;
- Addressing street connectivity as well as paving and widening of roads for evacuation routes; and

- Amending landscape ordinance requirements for maintenance of pervious surface areas for stormwater detention.

Wake County Transportation Plan (2003). The Wake County Transportation Plan focuses on unincorporated areas of Wake County, including many corridors that connect to and through Garner. One goal of the Garner Transportation Plan will be to match or recommend modifications to create a seamless future transportation system between the Town of Garner and Wake County. Ideally, this transportation system would be consistent or transition smoothly from one jurisdiction to another, particularly in relation to number lanes, lane widths, pedestrian facility connections, transit routes and bikeway connectivity.

There are some specific elements of the Wake County Transportation Plan that relate to Garner. The Outer Loop is one such facility, as is the “future transit corridor” identified along the US 401 / Norfolk Southern Corridor. The concept for this service shows transit activity centers designated in Garner, east of Garner and south of Garner near the proposed intersection of US 401 and the Outer Loop.

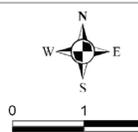
The Plan also indicates planned Intelligent Transportation System improvements along US 70 and US 401 through Garner. These improvements are designated to be for traffic signal control systems and Closed Circuit Television (CCTV) at the intersection of US 70 and US 401. The Plan also identifies CCTV installation as part of a freeway management system along the US 70 corridor east of Garner.



MAP 6



NOTES:
Amended May 3, 2004
Portion of Ackerman Road removed
All of Yeargan Road removed



- Garner Urban Services Area
- Garner Town Limits
- Thoroughfares**
- TYPE**
- Existing Freeways/Interstates
- Proposed Freeways/Interstates
- Existing Major Thoroughfares
- Proposed Major Thoroughfare
- Existing Minor Thoroughfares
- Proposed Minor Thoroughfare
- Non Jurisdictional
- Clayton Bypass
- Interchanges**
- Existing Interchange
- Proposed Interchange

November 16, 1999

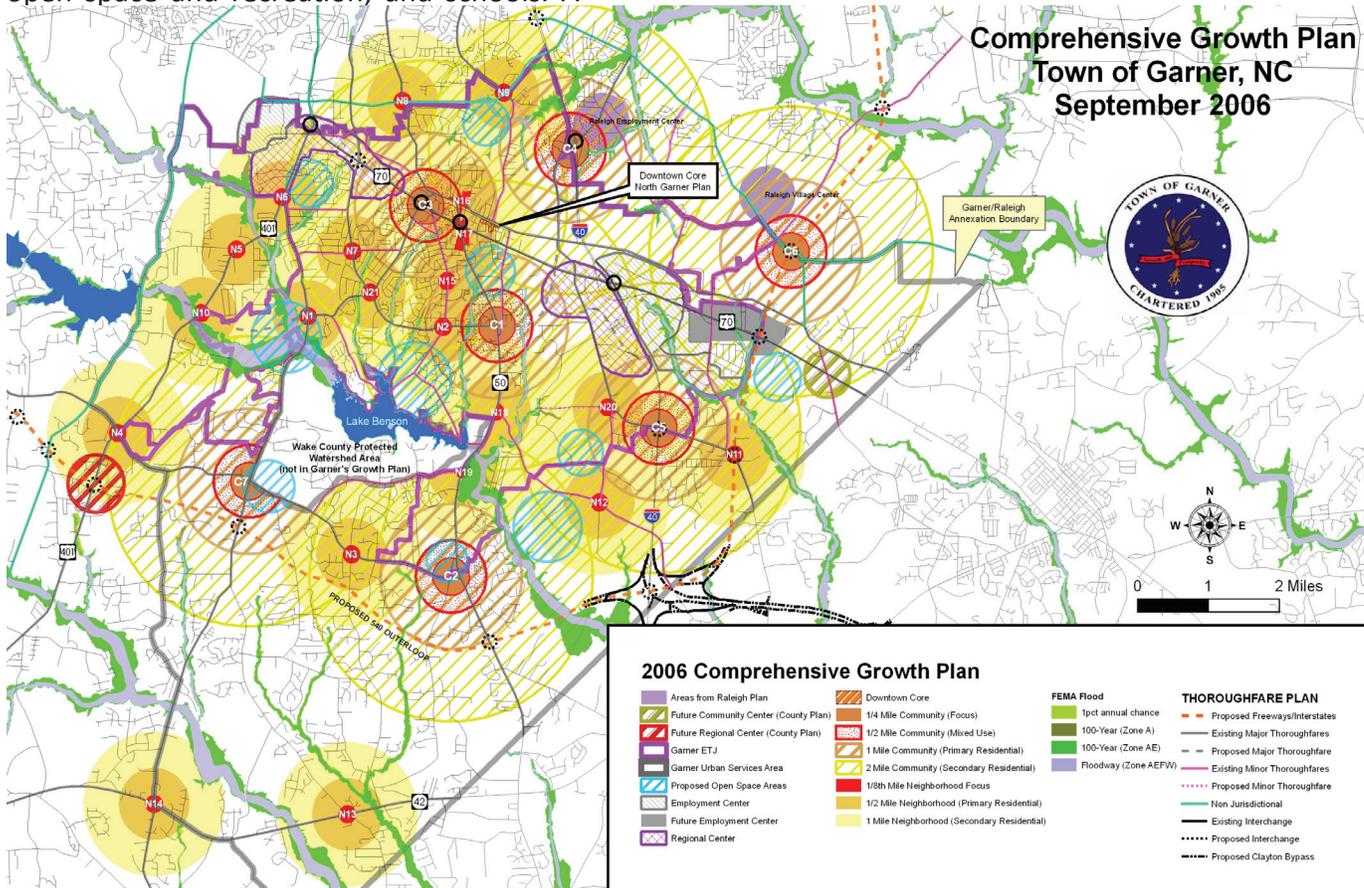
Thoroughfare Plan for the Town of Garner

Wake County
North Carolina

Wake County Growth Management Strategy (2003). One of the more impressive and comprehensive efforts undertaken by Wake County and all of its municipalities culminated in the Wake County Growth Management Strategy and report. The report covers a number of areas like governance and intergovernmental cooperation; land use and community character; open space and recreation; and schools. A

“preferred,” work to set minimum development densities and growth thresholds in part to reduce vehicle miles of travel;

- Tailor parking, pedestrian, and land use codes to promote mixed-use development in growth areas; and
- Develop and administer cooperative transportation strategies and plans between jurisdictions, the Capital Area Metropolitan Planning Organization (CAMPO), and



section is devoted to transportation as well, and identifies a number of recommendations that are still relevant to mainstream, integrative transportation planning today:

- Coordinating high-priority preservation areas with future transportation improvements to avoid conflicts and negative, secondary growth impacts;
- Conversely, in areas where growth is

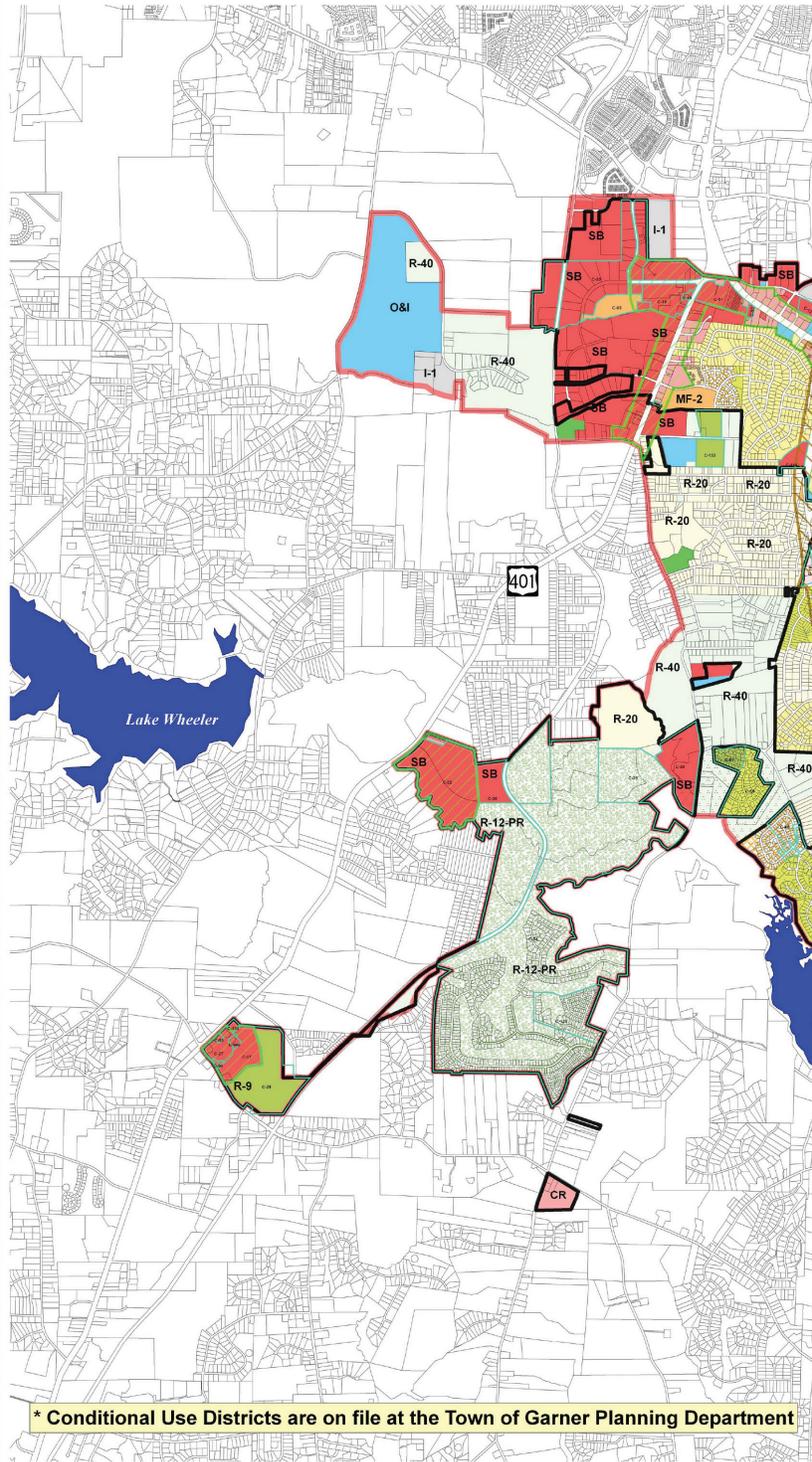
the state department of transportation that includes unincorporated areas of the county as well as a multimodal secondary street system.

Specific strategies like developing a new financing mechanisms for local road improvements and implementing a larger and more aggressive menu of travel demand options (e.g.,

carpooling, employer flex time scheduling) seem even more compatible with circumstances today than when they were written, given the current fiscal conditions in which state and federal transportation authorities find themselves. It is worth noting that when this report was written, counties in North Carolina did not carry the authority to purchase street rights-of-way or build and maintain transportation facilities. However, the authority to do so has since been granted by the N.C. State Legislature. Other relevant, current conditions such as the current economic recession that has forced many more people to turn to walking, bicycling and transit modes were not foreseen at the time of this committee.

North Carolina Railroad Shared Corridor Commuter Rail Capacity Study (2008). The North Carolina Railroad Company is in the process of completing a capacity study for a commuter rail service that would operate in a shared corridor with freight traffic from Greensboro to Goldsboro. The North Carolina Railroad is a private company owned by the State of North Carolina and extends from Morehead City to Charlotte. The proposed commuter rail service would operate along this corridor, with four different service segment anticipated along the corridor between Greensboro and Goldsboro. Garner is included in the eastern sector of the corridor in an 84-mile segments that would connect West Durham to Goldsboro. A preliminary schematic of the service indicates two stations in the Garner area (South Raleigh / West Garner and Garner Area), with an additional station shown for the future Outer Loop location east of Garner.

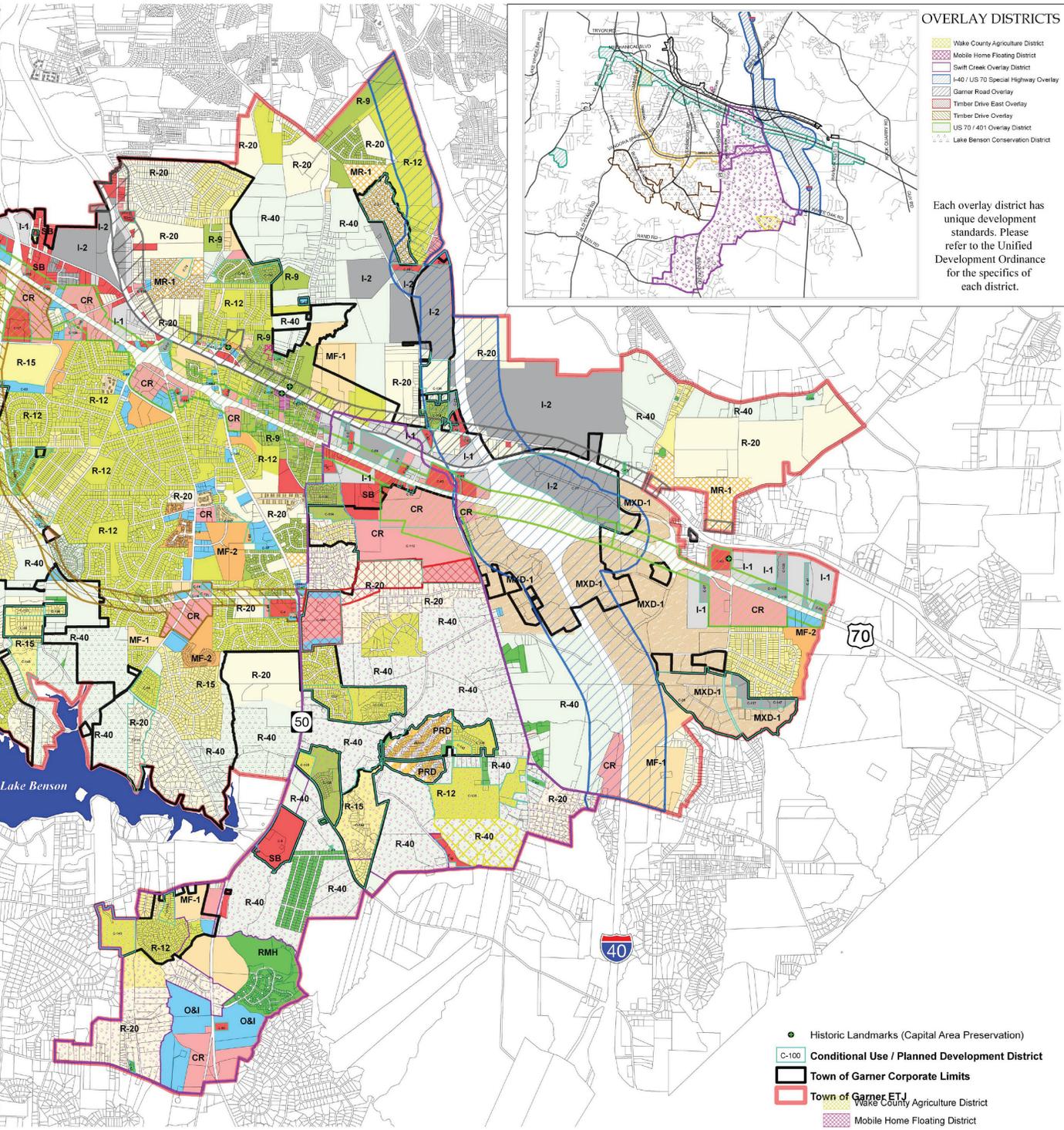
Zoning



Zoning Districts

 R-40 Single Family (40,000 s.f. lots)	 R-9 Single Family (9,000 s.f. lots)	 NO Neighborhood Office	
 R-20 Single Family (20,000 s.f. lots)	 RMH Manufactured Homes and Parks	 O&I Office & Institutional	
 R-15 Single Family (15,000 s.f. lots)	 MF-1 Multi Family 1	 CBD Central Business District	
 R-12 Single Family (12,000 s.f. lots)	 MF-2 Multi Family 2	 NC Neighborhood Commercial	

Current Garner Zoning



OVERLAY DISTRICTS

- Wake County Agriculture District
- Mobile Home Floating District
- Swift Creek Overlay District
- I-40 / US 70 Special Highway Overlay
- Garner Road Overlay
- Timber Drive East Overlay
- Timber Drive Overlay
- US 70 / 401 Overlay District
- Lake Benson Conservation District

Each overlay district has unique development standards. Please refer to the Unified Development Ordinance for the specifics of each district.

CR Community Retail **MXD Mixed Use Development**
SB Service Business **Planned Residential Development**
I-1 Industrial 1 **Traditional Neighborhood Development**
I-2 Industrial 2 **Planned Unit Development**

Obsolete Zoning Districts

- MXD-1 Mixed Use District
- MR-1 Multi Residential
- R-12-PR Planned Residential
- RCD-3 Residential Cluster

Obsolete districts have been mapped on the Official Zoning Map of the Town of Garner, but are no longer part of the zoning district hierarchy applied to the Town. These districts have been retained from the previous version of the land use ordinance, and land that is zoned with any of the obsolete district designations will continue to be developable under those use and dimensional regulations applicable to the districts immediately prior to the effective date of this UDO. See Section 4.1 entitled Establishment of Districts for list of obsolete districts.

- Historic Landmarks (Capital Area Preservation)
- C-100 Conditional Use / Planned Development District
- ▭ Town of Garner Corporate Limits
- ▭ Town of Garner ETJ
- Wake County Agriculture District
- Mobile Home Floating District
- Swift Creek Overlay District
- I-40 / US 70 Special Highway Overlay
- Garner Road Overlay
- Timber Drive East Overlay
- Timber Drive Overlay
- US 70 / 401 Overlay District
- Lake Benson Conservation District
- Swift Creek Overlay Non Exempt Areas

Although not mapped, existing residential and commercial uses in place before June 1, 2005 that are within the Swift Creek Overlay boundary are exempt from the development standards.

Design Process

Understanding the Design Process

IN EVERY SUCCESSFUL DESIGN PROCESS clear steps must be outlined along with achievable benchmarks that can move a community from cloud of ambiguity to the clarity of a community based master plan.

The key element that threaded throughout the plan was an emphasis on community involvement within the design process. The foundation of Garner's Downtown Streetscape Master Plan has

Process Overview

- Guiding Principles
- Identify Project Limits and Focus Areas
- Review of Existing Conditions
- Develop a Project Vision
- Develop Project Goals
- Challenges and Opportunities
- Conceptual Development
- Conduct Public Workshop
- Development of Conceptual Alternatives
- Development of the Preferred Alternative
- Conduct Public Workshop
- Preliminary Master Plan Document
- Final Master Plan Document

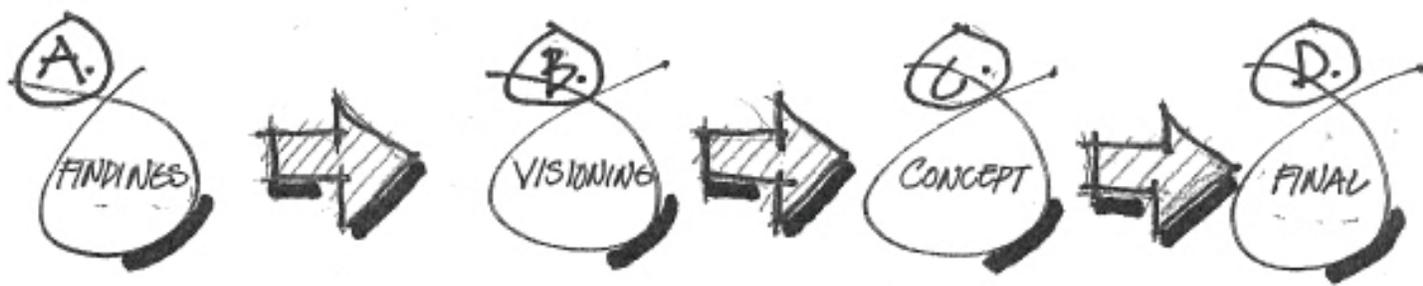
been the value of workshops and the charrette process as an instrument that can break barriers and bring all people of diverse backgrounds together. This problem-solving process utilized in the development of the master plan established a clear vision along with identifying opportunities and constraints from which the project design alternatives would be developed. This master plan

is truly a canvas of various elements static until all issues are brought to light. This process was a highly engaging and visual exercise where issues, ideas, and thoughts were captured instantaneously. Images were clearly portrayed and understood easily by both professional and citizenry alike and reflect the unique qualities of the project and its cultural and physical environment. The outcome of the Garner's Downtown Streetscape Master Plan process was a clearly defined vision, an understanding of opportunities and constraints, and the development of a consensus based master plan that addressed project principles.

Guiding Principles

AS A POINT OF BEGINNING the Downtown Streetscape Master Plan had several guiding principals. These principals provide a list of requirements that the master planning process must achieve and whose outcome the plan will ultimately be judged. The following are a list of the master plans guiding principles.

- It is important for Garner to engage its community within the process
- Master Plan must have design detail sufficient enough to move into final design
- G.R.A. (Garner Revitalization Association) will play an important role in the steering committee for the project
- An alternative is to develop an area of emphasis within the project area in order to provide sufficiently detailed direction for the project area.



- Project must be sufficiently completed and cost estimates developed by the end of the calendar year for budgeting
- Garner will need a phasing component to the master plan
- Berger should consider transit-oriented development options
- The Streetscape Master Plan should begin to articulate Design Guide Lines
- Gateways should be looked at as described in the North Garner Small Area Plan
- The Garner Road Core is important; the community is looking for timely improvements

Project Limits and Focus Areas

THE STUDY AREA LIMITS as delineated by the project steering committee are: Benson Roads (west); Jones Sausage Road (east); US 70 between Benson Road and New Rand Road (south); and the right-of-way for Garner Road (north). Focus areas include:

1. Downtown “core” (Emory to Purvis Streets) to the railroad;
2. Intersections of Garner Road/Benson Road;
3. Intersections of US 70/Benson Road;
4. Intersections New Rand Road/US 70; and
5. Intersections of New Rand Road/Main Street.



Visioning

A KEY COMPONENT OF THE MASTER PLANNING PROCESS was the VISIONING Workshop where the direction of the project was established by the Steering Committee.

A problem statement was presented to the Steering Committee as follows: What design considerations should be part of Garner's Downtown Streetscape Plan.

The group then divided into pairs and began brainstorming and writing down ideas that applied the problem statement. The ideas were then categorized and prioritized. The categories were as follows:

- Connectivity
- Being 'Green'
- Utilities
- The Visual Message

Design Opportunities Cards

- Develop Landscape and Hardscape Standards
- Link the Community Together
- A Place for Programmed Events
- Brand Downtown
- Downtown has Only Three Current Destinations Now
- Improve access
- Tie to way finding-NCDOT
- Improve interpretation
- Bike connections
- Parks and Greenspace
- Openspace connect with Downtown
- Expand Development
- Local history
- Signage: Historic/ Interpretive

Uplifting, Restoring, and

- Financing
- Revitalization

At the end of the Visioning Session a summary vision statement was developed by the group. The statement was as follows: Uplifting, Restoring, and Evolving Downtown Garner.

Opportunities and Challenges

DURING THE VISIONING WORKSHOP the Steering Committee participated in an Opportunities and Challenges Session in order bring to light all aspects of the design of a streetscape for Downtown Garner. Opportunities and Challenges were further divided into Design and Planning. The text boxes on this page and the next detail the outcomes of the session.

Design Challenges Cards

- Rail Road
- Linkages and Gateways
- Downtown Identity
- Our Message
- NCDOT
- Diversity of Landscapes

Planning Opportunities Cards

- Stimulus Package
- Encourage Business and Consumer Investment
- Tie the Community Together
- Programmed Events
- New Governor
- Activate Community
- Nearby Advocate-Ross
- Connection with broader tourism
- Raise Awareness of Town Involvement
- Become a model for other Communities
- Increase Property Value
- Leverage Private Investment
- Change Negative Image
- Live-Work-Play
- Green approach
- Small victories-project priorities
- Harness Private Investment
- Streetscape should be “better that good enough”



Steering Committee Members working through the Visioning Workshop

Evolving Downtown Garner

Planning Challenges Cards

- Funding
- Budgeting
- Community Opinion
- Balance with Town Priorities
- Getting and Retaining Businesses
- Community “buy-In”
- Private Development
- Rental Properties
- Getting Our Message Out
- Mediating Business Interruption
- Rental Properties
- Getting beyond “We Can’t”
- Stigma of Stagnation
- Poor economic timing



Chris Walsh reviewing elements of a streetscape

Conceptual Workshop

THE CONCEPTUAL WORKSHOP IS THE BRIDGE BETWEEN VISIONING AND THE FINAL PLAN of Garner's Downtown Streetscape. This is where ideas begin to take physical form. This transitional phase seeks to refine the Visioning Categories of Connectivity, Being 'Green, Utilities, The Visual Message, Financing, and Revitalization. These categories were then grouped into three themes: *Historic, Green, and Linkages*. The Steering Committee was then divided into three

Visioning Categories

- Connectivity
- Being Green
- Utilities
- The Visual Message
- Financing
- Revitalization

groups, one per each theme. The groupings were based upon each committee member's interest and expertise. As a starting point *Design Considerations* of each

Theme were presented. These considerations were derived from the Visioning workshop and served as a point of discussion within the groups.

Each group was facilitated by a member of Berger's or Garner's Staff who assisted with base maps to develop a master plan from each particular focus theme. After the plans were developed each group presented its concept to the steering committee followed by questions and answers. Programmatic items of each theme were documented.

Items that were not common to all themes were then listed and discussed to determine their inclusion within the Composite Master



Town Manager Hardin Watkins at a Public Workshop

Design Considerations



Historic Groupings

- Town Center
- Residential
- Rural
- Train and Station
- Historic Business

Sample Historic Menu

- Interpretative Signage
- Furnishings
- Lighting
- Hardscape Selection
- Landscape Groupings
- Destinations
- Themed events

Green Groupings

- Garner Road
- Main Street
- Benson Road
- New Rand Road

Sample Green Menu

- Sustainable Design
- Stormwater & Utility
- Green Infrastructure
- Identify Green Space
- Expand Greenway
- Bicycle Lanes
- Extensive Tree Plantings
- Trash and Recycling
- Informational Signage
- Materials List
- Leed Buildings

Linkage Groupings

- Town Center
- Neighborhoods
- Rural
- Highways
- greenways
- existing sidewalks

Sample Linkage Menu

- Key Facilities
- Gateways
- Connect Sidewalks
- Bicycle lanes
- Directional Signage
- Public Transportation
- Identify Light Rail Station

Historic Design Theme

THE PROGRAMMATIC AND DESIGN ELEMENTS of Garner's Downtown Streetscape as taken from the workshop are as follows:

- Sidewalks along Main Street
- Could incorporate logo along sidewalk to denote Historic Trail
- Cobblestone or brick surface along sidewalks on Main Street
- Remove cobra lights along Main Street
- Move existing lamp post along Residential Portion of Main Street
- Add lamp post that can accommodate ban-

- Furnishings along Main Street including: benches, bike racks, trash receptacles (could be developed by local artisan)
- Landscape groupings along Garner Road Corridor
- Tree plantings along Main Street-should not block view of business along commercial section
- Move Depot/ Caboose to park or public openspace
- Need additional parking for commercial section
- Signage program for historic buildings and



Allen and Betsy Haywood

Allen Haywood was a schoolteacher in Garner and important member of the African-American community in downtown. Their family home is still located on Main Street.

- ners along commercial section of Main Street-lights must compliment existing features
- Add two lamp posts with banners at the intersections of Main and Benson and Main and New Rand Roads in conjunction with "Friends of Garner" gateway signs

- significant features (bronze signage)
- Investigate cell phone walking tour
- Light Rail / Commuter Rail Stations located either at Jones Sausage and Garner Road or between Saint Marys Street and Benson Road
- Need connectivity on New Rand Road
- Sidewalks from Senior Center to Jones Sausage Road

Green Design Theme

THE PROGRAMMATIC AND DESIGN ELEMENTS of Garner's Downtown Streetscape as taken from the workshop are as follows:

- Develop multi-use trails connecting residences to business and parks
- Add multi-use trail along Garner Road between Benson and New Rand Road
- Add multi-use trail along Main Street
- Add pedestrian crossing at Benson and New Rand Road
- Eliminate parking along business store fronts adding multi-use trails
- Pedestrian / multi-use trail along Benson Road and the under the Rail Road overpass
- Pedestrian crossings along St Marys at Garner Road and Main Street
- Pedestrian crossings along New Rand Road at Garner Road and Main Street
- Link north and south Garner across US 70 via multi-use trail along Benson Road-tie to future greenspace on south side
- Site furnishings along Main Street and trail heads to include: bike racks, benches, and trash receptacles
- Tie multi-use trail to North Garner current



- Sidewalk/ trail enhancements for walking and biking on semi-impervious/ porous paving
- Recycling facilities at existing park
- Multi-use trail with trail heads at park facilities with parking and bike racks
- Develop planting scheme between Benson and New Rand Roads along the island on US 70
- and proposed greenway trails including Old School Common Trail and Proposed Park Greenway
- Develop a stormwater management plan
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Add pedestrian crossing at intersections
- Multi-use trail system along Main Street
- Light Rail/ Commuter Rail Stations located between Saint Marys Street and Benson Road

Linkages Design Theme

THE PROGRAMMATIC AND DESIGN ELEMENTS of Garner's Downtown Streetscape as taken from the workshop are as follows:

- Expand bus route from US 70 to hwy 50 Benson Road to Garner Road to New Rand Road back to hwy 70 to White Oak
- Add transit stop at Senior Center
- Develop bike routes connecting residences to business and parks
- Add bike lanes along Garner Road
- Develop transit gateway at New Rand Road and US 70 to provide crossing from north to south ends of town for pedestrians and bikes
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Add pedestrian crossing at intersections
- Sidewalk system along Main Street
- Develop decorative directional signs
- Expand Pearl from Main Street to US 70
- Add curb and gutter and sidewalks along Pearl and Montague Streets
- Add bike lanes to connect to greenways
- Connect parks with sidewalks
- Add bike lane along Benson Road include bike lanes on bridge replacement
- Transportation Plan should we have Multi-use paths or separate bike lanes?
- Add curb and gutter and underground utilities along New Rand Road
- Install bike racks on Main Street
- Add signs at US 70 and Benson, Pearl, and Montague Streets



- Develop new street lamps
- Light Rail/ Commuter Rail Stations located between Saint Marys Street and Benson Road

Composite Design Theme

THE PROGRAMMATIC AND DESIGN ELEMENTS of Garner's Downtown Streetscape as taken from the workshop are as follows:

- Sidewalks along Main Street
- Could incorporate logo along sidewalk to denote Historic Trail
- Cobblestone or brick surface along sidewalks on Main Street
- Remove cobra lights along Main Street
- Move existing lamp post along Residential Portion of Main Street
- Add lamp post that can accommodate banners along commercial section of Main Street-lights must compliment existing features
- Add two lamp post with banners at the intersections of Main and Benson and Main and

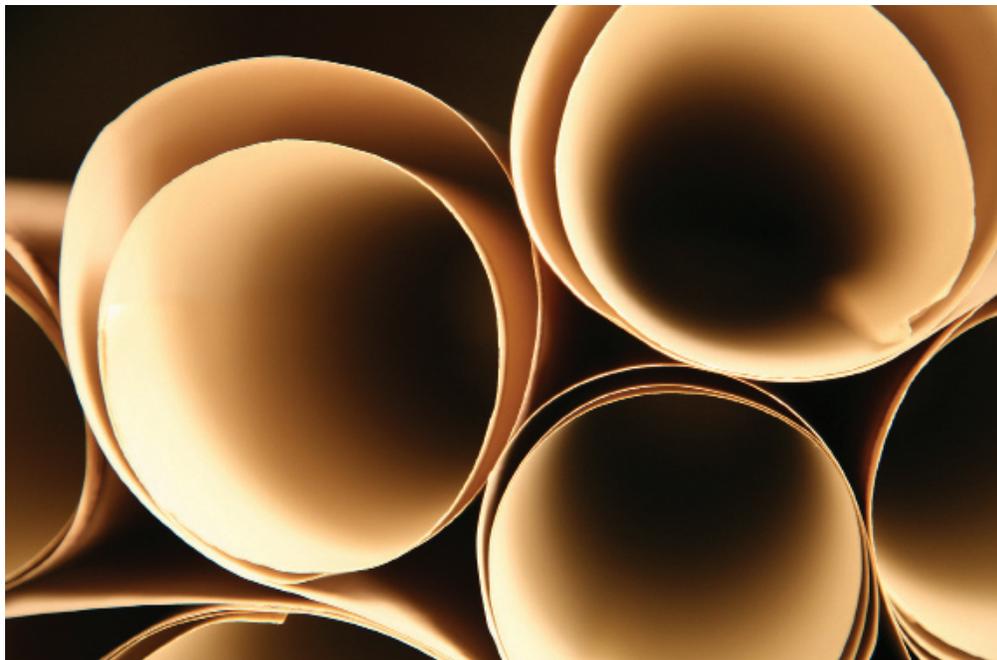
New Rand Roads in conjunction with "Friends of Garner" gateway signs

- Furnishings along Main Street including: benches, bike racks, trash receptacles (could be developed by local artisan)
- Landscape groupings along Garner Road Corridor
- Side walks from Senior Center to Jones Sausage Road
- Signage program for historic buildings and significant features (bronze signage)
- Recycling facilities at existing park
- Multi-use trail with trail heads at park facilities with parking and bike racks
- Pedestrian crossings along

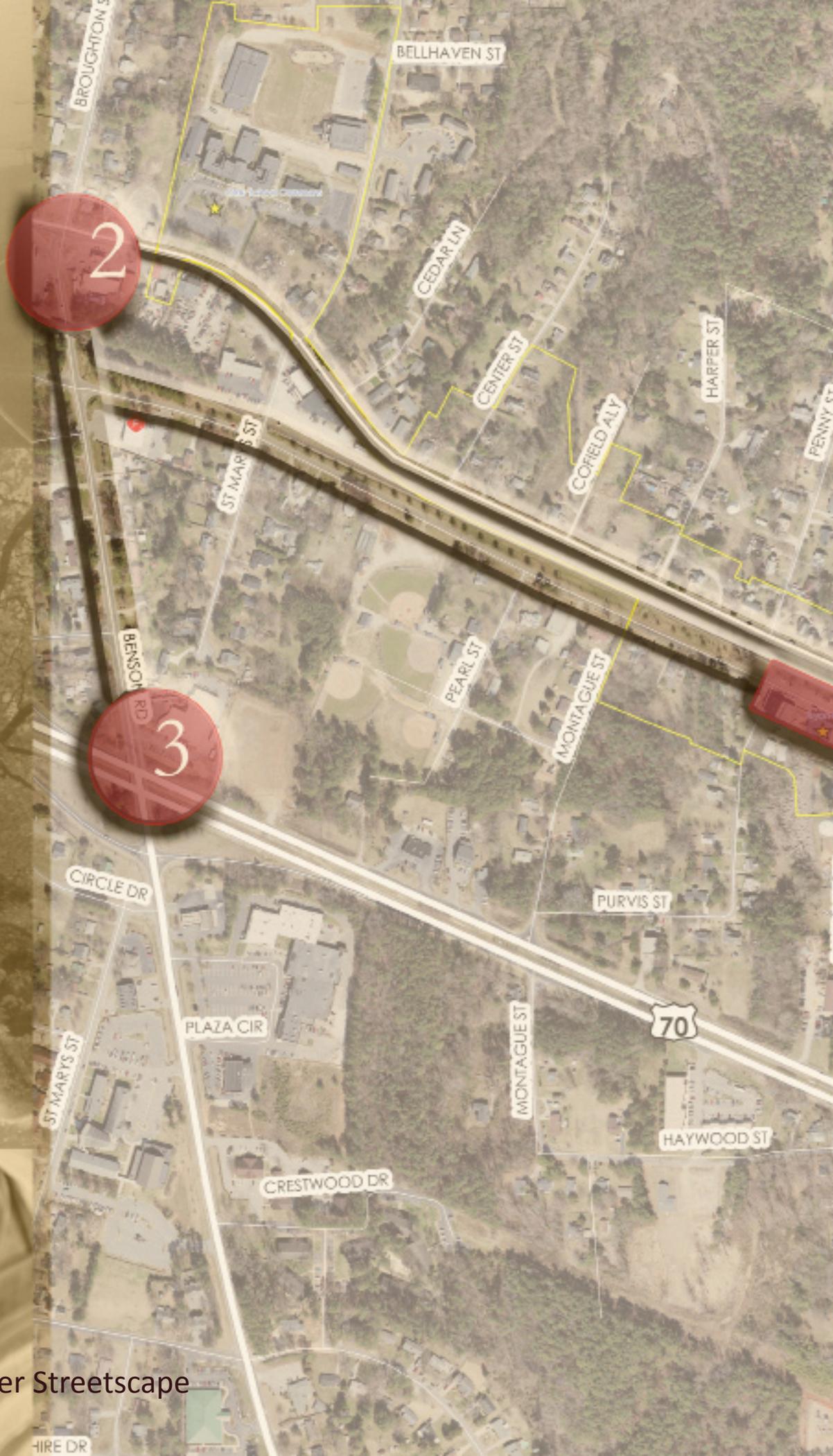
St Marys at Garner Road and Main Street

- Pedestrian crossings along New Rand Road at Garner Road and Main Street
- Link north and south Garner across US 70 via multi-use trail along Benson Road-tie to future greenspace on south side
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Add pedestrian crossing at intersections
- Develop planting scheme between Benson and New Rand Roads along the island on US 70
- Sidewalk/ trail enhancements for walking and biking on semi-impervious/ porous paving-should be considered
- Expand bus route from US 70 to hwy 50 Benson Road to Garner Road to New Rand Road back to hwy 70 to White Oak
- Add transit stop at Senior Center
- Develop bike routes connecting residences to business and parks
- Add bike lanes along Garner Road
- Develop transit gateway at New Rand Road and US 70 to provide crossing from north to south ends of town for pedestrians and bicyclists
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Develop decorative directional signs

- Add bike lanes to connect to greenways
- Connect parks with sidewalks
- Add bike lane along Benson Road include bike lanes on bridge replacement
- Light Rail/ Commuter Rail Stations located between Saint Marys Street and Benson Road-should be considered
- Green lighting(LED)- should be considered
- Public Art should inform the street furniture
- Black wrought iron theme for furnishings throughout plan
- Pavers on Main Street pavers over buried utilities-show alternative materials and cost comparisons
- Stormwater problem areas-clean water trust fund money
- Appropriate plantings along each unique area
- Need parking study for main street
- Sharrows if bike lanes are not possible
- Transportation plan needs to determine bike lanes or multi-use trail
- Transportation plan needs to determine if there should be round-a-bouts at intersections of New Rand Road and Garner Road and Benson and Garner Road
- Move depot and caboose to Garner Recreational Park



Downtown Streetscape Master Plan



Master Plan-Highlight Elements

- Sidewalks along Main Street
- Cobblestone or brick surface along sidewalks on Main Street
- Move existing lamp post along Residential Portion of Main Street
- Add lamp post that can accommodate banners
- Furnishings along Main Street
- Landscape groupings along Garner Road Corridor
- Side walks from Senior Center to Jones Sausage
- Recycling facilities at existing park
- Multi-use trail with trail heads at park facilities with parking and bike racks
- Pedestrian crossings along St Marys at Garner Road and Main Street
- Pedestrian crossings along New Rand Road at Garner Road and Main Street
- Link north and south Garner across US 70 via multi-use trail along Benson Road-tie to future greenspace on south side
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Expand bus route from US 70 to hwy 50 Benson Road to Garner Road to New Rand Road back to hwy 70 to White Oak
- Add sidewalks from US 70 along New Rand Road to Garner Road
- Develop decorative directional signs
- Add bike lanes to connect to greenways
- Connect parks with sidewalks
- Add bike lane along Benson Road include bike lanes on bridge replacement
- Black wrought iron theme for furnishings throughout plan
- Move depot and caboose to Garner Recreational Park
- Widen New Rand Road from ten foot to eleven foot lanes
- Widen New Rand at US 70 to accommodate turn lane
- Curb and gutter New Rand from US 70 to Main Street
- Add sidewalk system along New Rand Road
- Add streetscape planting along New Rand Road
- Add decorative street lighting to match Main Street



Main Street

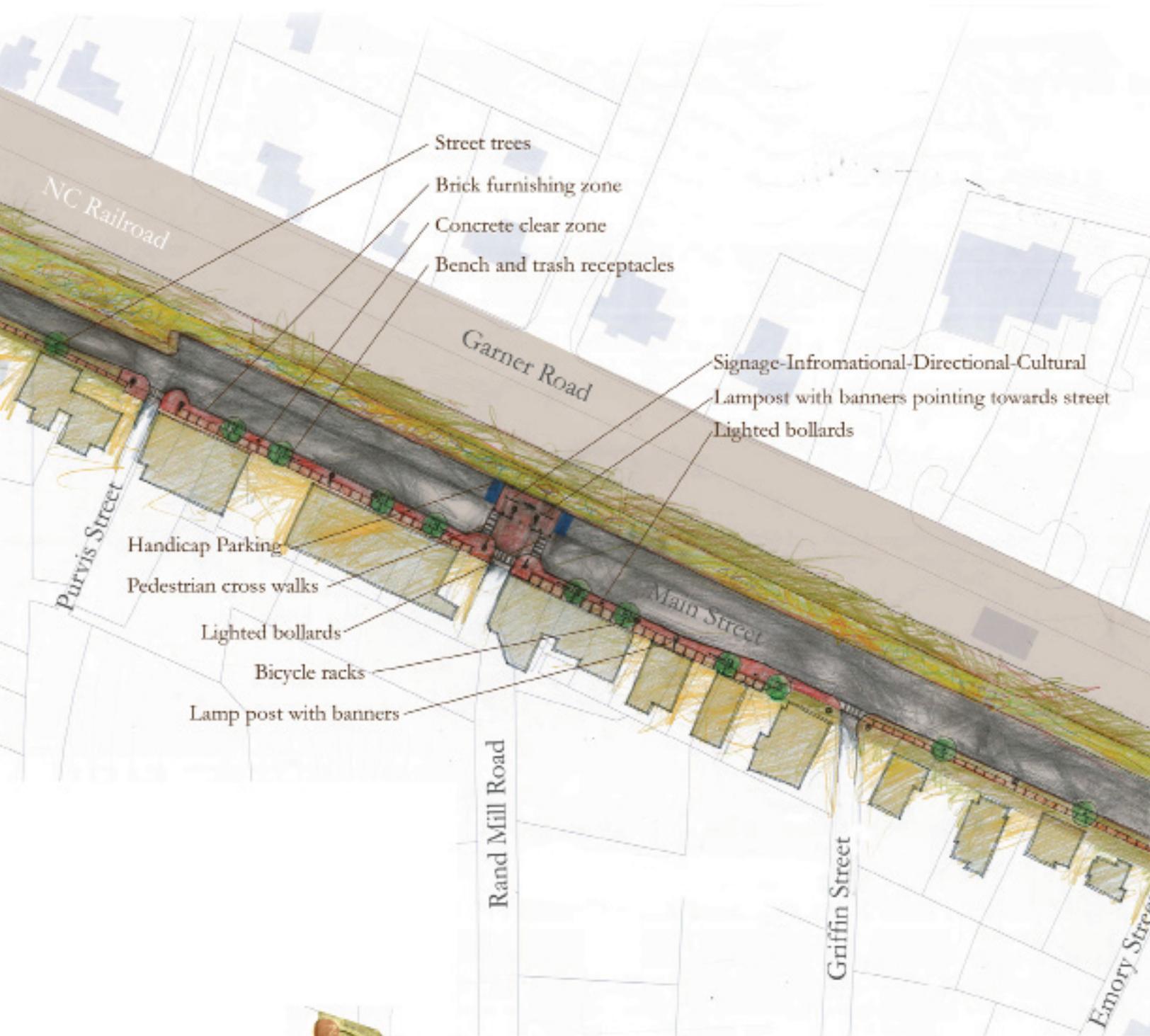
THE DESIGN ALONG MAIN STREET TRANSFORMS the existing streetscape into a visual spine between New Rand and Benson Road. This visual spine is a town scaled boulevard with a community gathering place in its center. The visual cues of both hardscape and landscape along the spine as well as at the intersections of New Rand and Benson Roads emphasize Main



Street as the prominent historic and cultural street in the Town of Garner. These improvements will draw local citizens and visitors to the historic town center and the community square and gathering space. This new town square is an important community space that provides the Town of Garner a location for special events, Informational, Directional, and Cultural Signage. In addition this space acts as a speed table providing traffic calming measures as well as increasing automobile and foot traffic for local business along Main Street.

Site Program

- Delineate sidewalk into nine foot clear zone and five foot furnishing zone
- Brush sweep and score concrete clear zone and brick furnishing zone
- Add one foot concrete base for lamp post
- Relocate existing lamp post adding lighted bollards, benches, trash receptacles, bike rack, and post box
- Add street trees and metal tree grates
- Create brick welcoming platform/speed table with lighted bollards, lamp post, and informational, directional, and cultural signage
- Add crosswalks and handicap parking adjacent to welcoming platform
- Restripe existing parking from angular to perpendicular
- Plant wild flowers with 70% yellow mix between railroad and Main Street

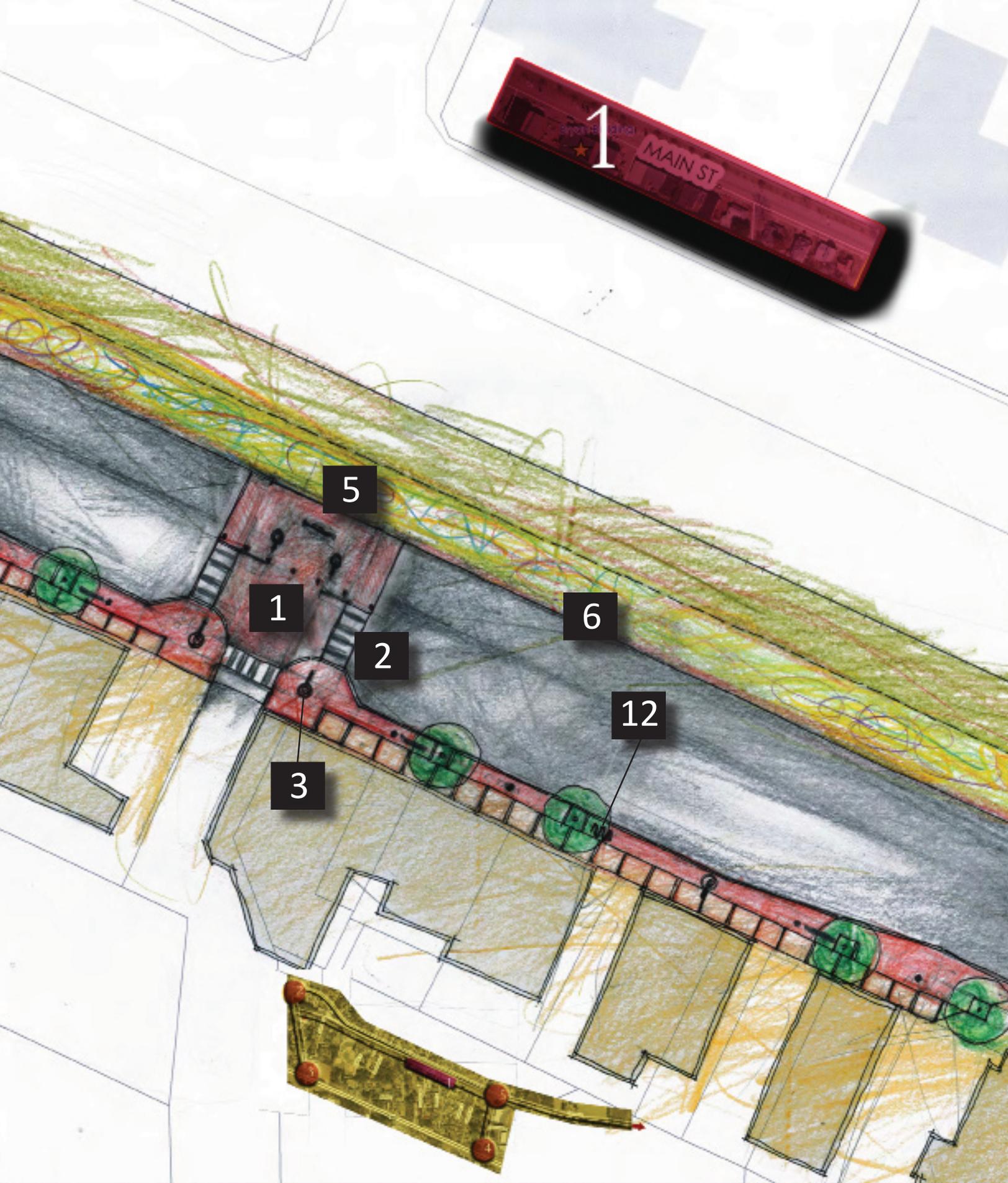


Detail of Main Street

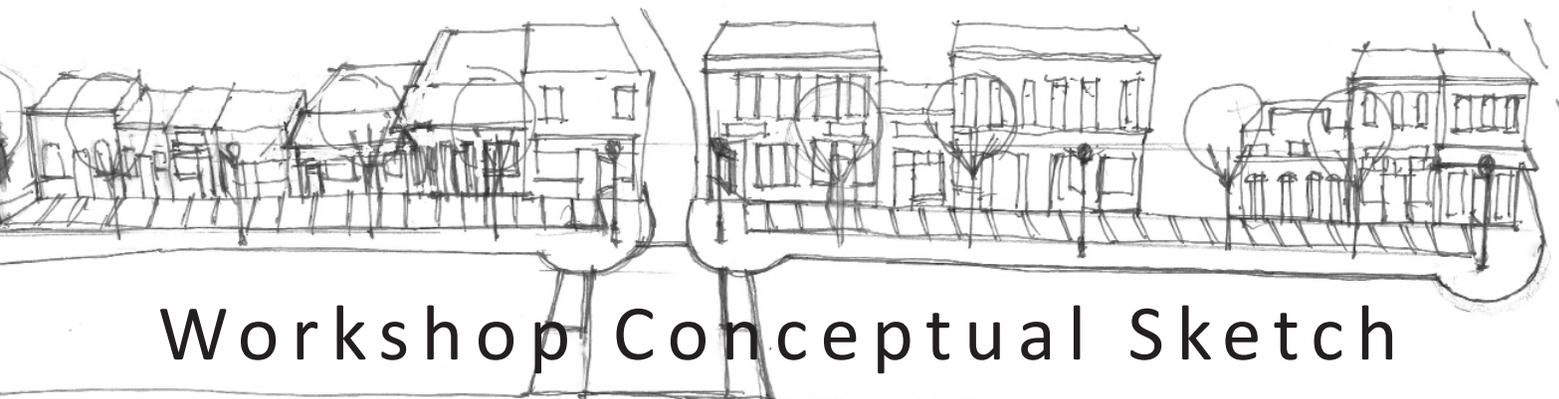
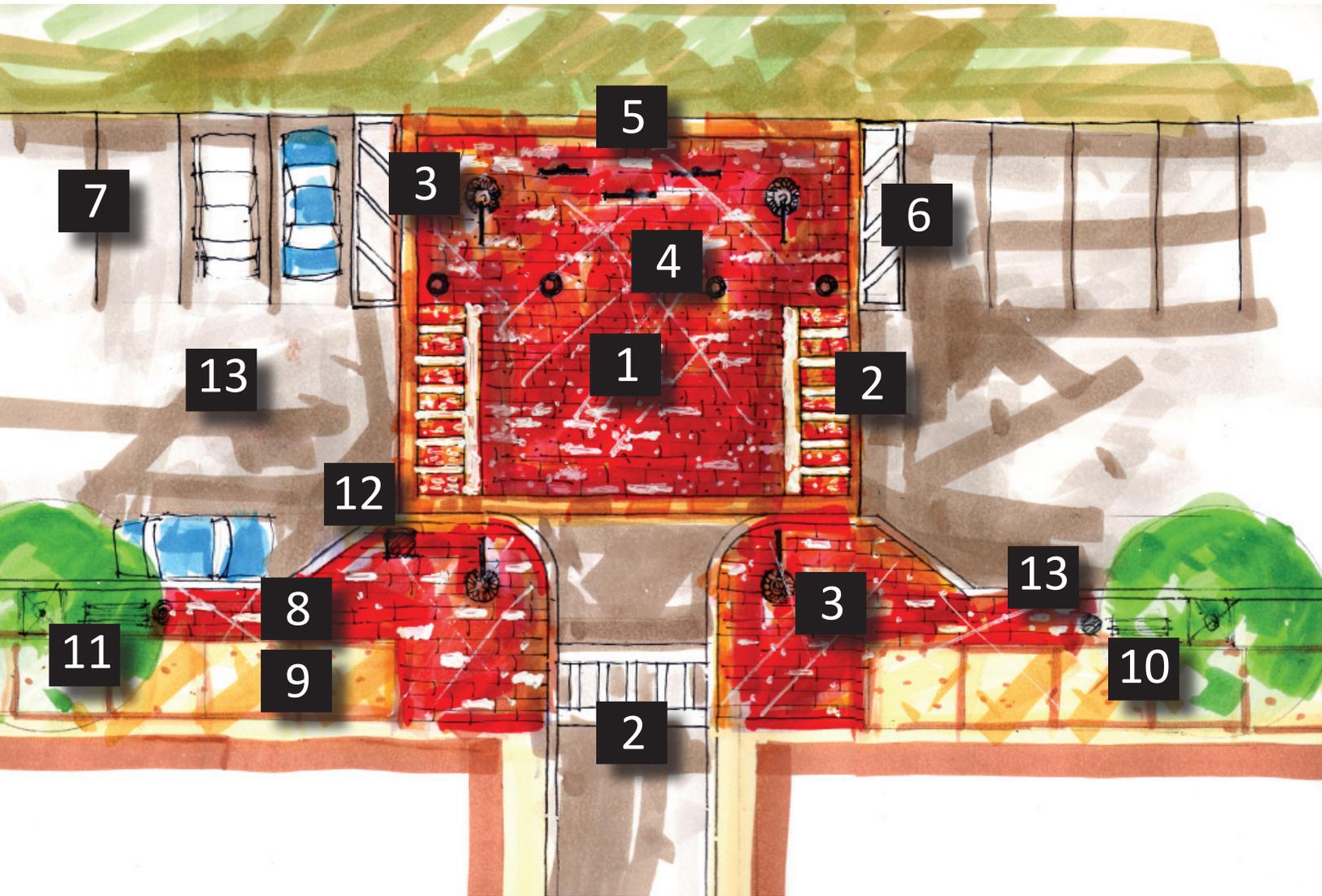


Elements of Main Street

1. Town Square/ Speed Table
2. Pedestrian Crosswalk
3. Light Pole with Banner (typ.)
4. Lighted Bollards (typ.)
5. Interpretative Signage
6. Perpendicular Parking
7. Brick Furnishing Zone
8. Concrete Clear Zone
9. Bench (typ.)
10. Trash Recepticals (typ.)
11. Street Trees (typ.)
12. Bicycle Rack (typ.)



Detail of Main Street Square



Workshop Conceptual Sketch of Main Street

Elements of Town Square

1. Town Square / Speed Table
2. Pedestrian Crosswalk
3. Light Pole with Banner (typ.)
4. Lighted Bollards (typ.)
5. Interpretative Signage
6. Handicap Parking
7. Perpendicular Parking
8. Brick Furnishing Zone
9. Concrete Clear Zone
10. Bench (typ.)
11. Tree Grates (typ.)
12. Curb and Gutter
13. 11' Travel Lanes



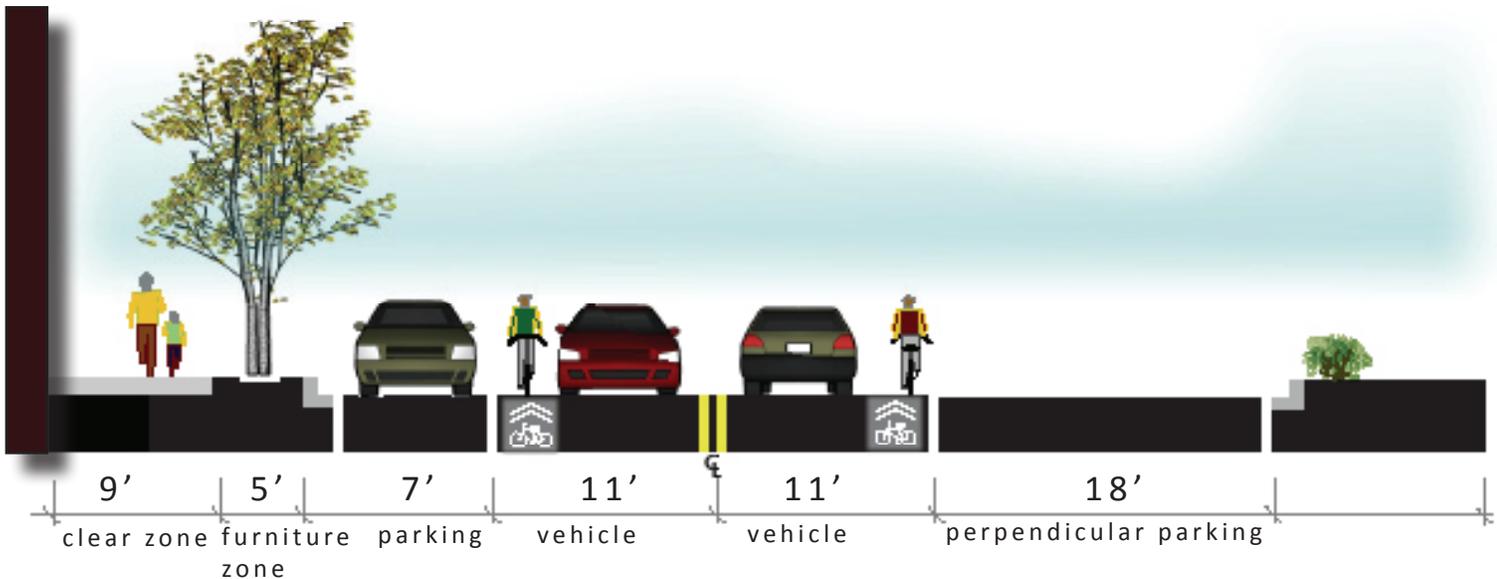
View of Main Street

Elements of Section

1. 11' Travel Lanes
2. 18' Parking Space
3. Parallel Parking
4. Street Tree with Grate
5. Bench



Corner of Main Street
and Rand Mill Road



Section of Main Street

Benson Road

The following represents the site program for Benson Road and Main Street. Benson Road is an important Linkage to Main Street from US-70 and its connection to West Garner Road. The north leg of the Garner/Benson intersection is Broughton Road, which connects directly to North Garner Middle School. School buses approaching this intersection during the afternoons on school days face a difficult right-turn now because of the existing geometry. Redesigning the southeast corner of this same intersection could dramatically improve its appearance and provide more usable area for the venerable Toot-n-Tell Restaurant, a landmark place in Garner and the

historic downtown.

Several key elements of the streetscape design in the focus include: improvements to the intersection of Benson Road and Main Street to draw attention and interest towards Main Street, sidewalks along the entirety of Benson along with a retaining wall underneath the NCRR Bridge to facilitate a connection to West Garner Road, and historic, destination, and informational signage along this corridor. Additionally, curb-and-gutter is proposed for the entire corridor along with streetscape plantings.

Site Program

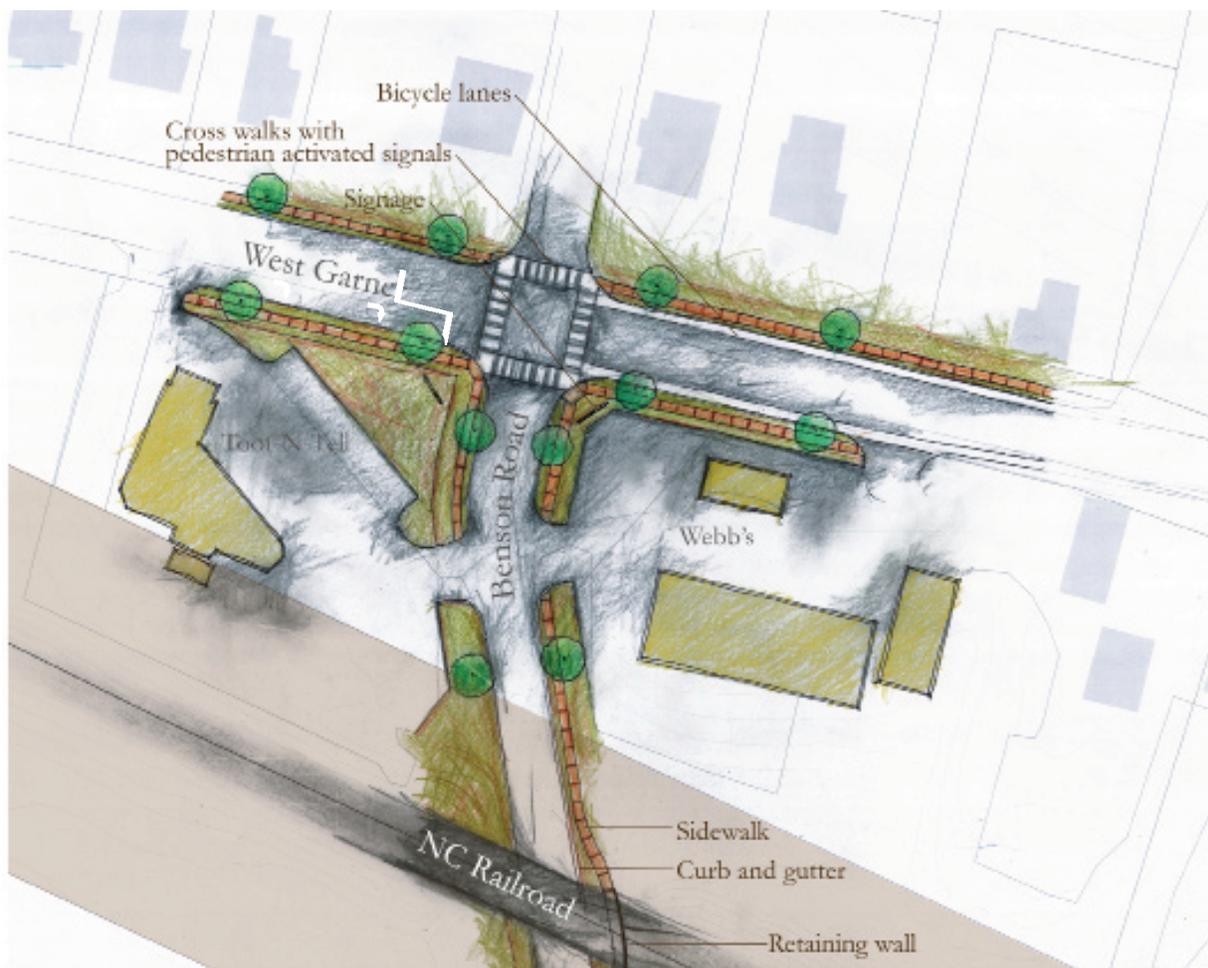
Benson at West Garner

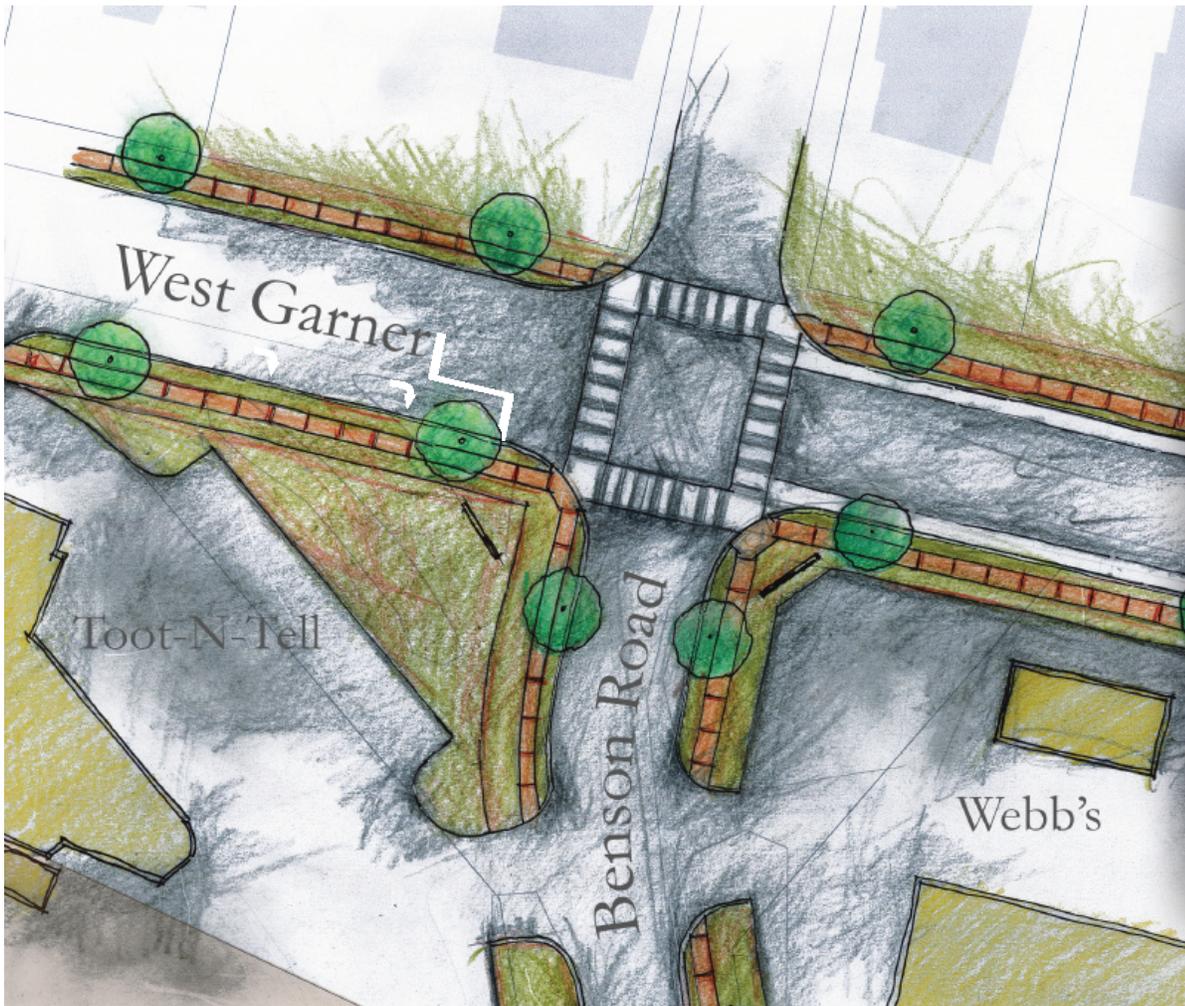
- Develop a sidewalk system south of West Garner along Benson Road
- Add chokers/ bulb-outs reducing high-speed right turns
- Install gateway/ directional signage
- Install pedestrian cross walks and activated signals at all corners
- Install bicycle lanes along Garner Road beginning at the Benson intersection
- Install destination and gateway signage at both corners of Benson and West Garner intersection
- Install curb-and-gutter along Webb's gas station
- Reduce driveway sizing along Toot-N-Tell
- Add curb-and-gutter along Benson Road from Main Street to West Garner
- Add retaining wall under NC Railroad bridge to facilitate sidewalk
- Add streetscape planting
- Add black mast arm signal support poles (w/piers)
- Dedicated right turn lane on the eastbound approach of Garner Road at intersection at Benson
- Move stop "bar" for eastbound through approach back to allow for better right turns of school buses making a right turn from the southbound direction

Site Program

Benson at Main Street

- Install Historic Downtown Garner signage
- Add curb and gutter along Benson Road from Main Street to West Garner
- Add streetscape planting
- Add four lamp post on each side of intersection (with approved by NCDOT and NCRR, as necessary)
- Add five-foot sidewalks with a four-foot grass strip along Main Street
- Plant wild flowers with a seventy percent yellow mix between railroad and Main Street





Decorative Traffic Signal Support Poles at West Garner and Benson Road Intersection





Benson Road looking towards Garner Road



Benson Road under NCRR Bridge



Garner Road looking towards Benson Road



Benson Road looking towards NCRR Bridge and Main Street



looking towards Webb's Gas Station



Benson Road at US-70

The following represents the site program for Benson Road at US 70 and is an important gateway to Downtown Garner. Several key elements included in the streetscape design

Site Program

- Extend sidewalk system on east side of Benson Road to Main Street
- Add directional signage north of bridge
- Add Gateway signage
- Add pedestrian crosswalks north of bridge
- Add streetscape planting north of bridge along both sides of Benson Road
- Eliminate fast right turns south of bridge and add cross walks with pedestrian activated signals
- Add sidewalk system south of bridge on the east side
- Future replacement bridge-add bike lanes and eight foot multi-use sidewalk on east side of bridge

are: The elimination of fast right turns south of the bridge adding cross walks with pedestrian activated signals, extending the current sidewalk system on east side along entire corridor, adding signage that includes both gateway and directional signage north of bridge, and adding streetscape planting north of bridge along both sides of Benson Road.



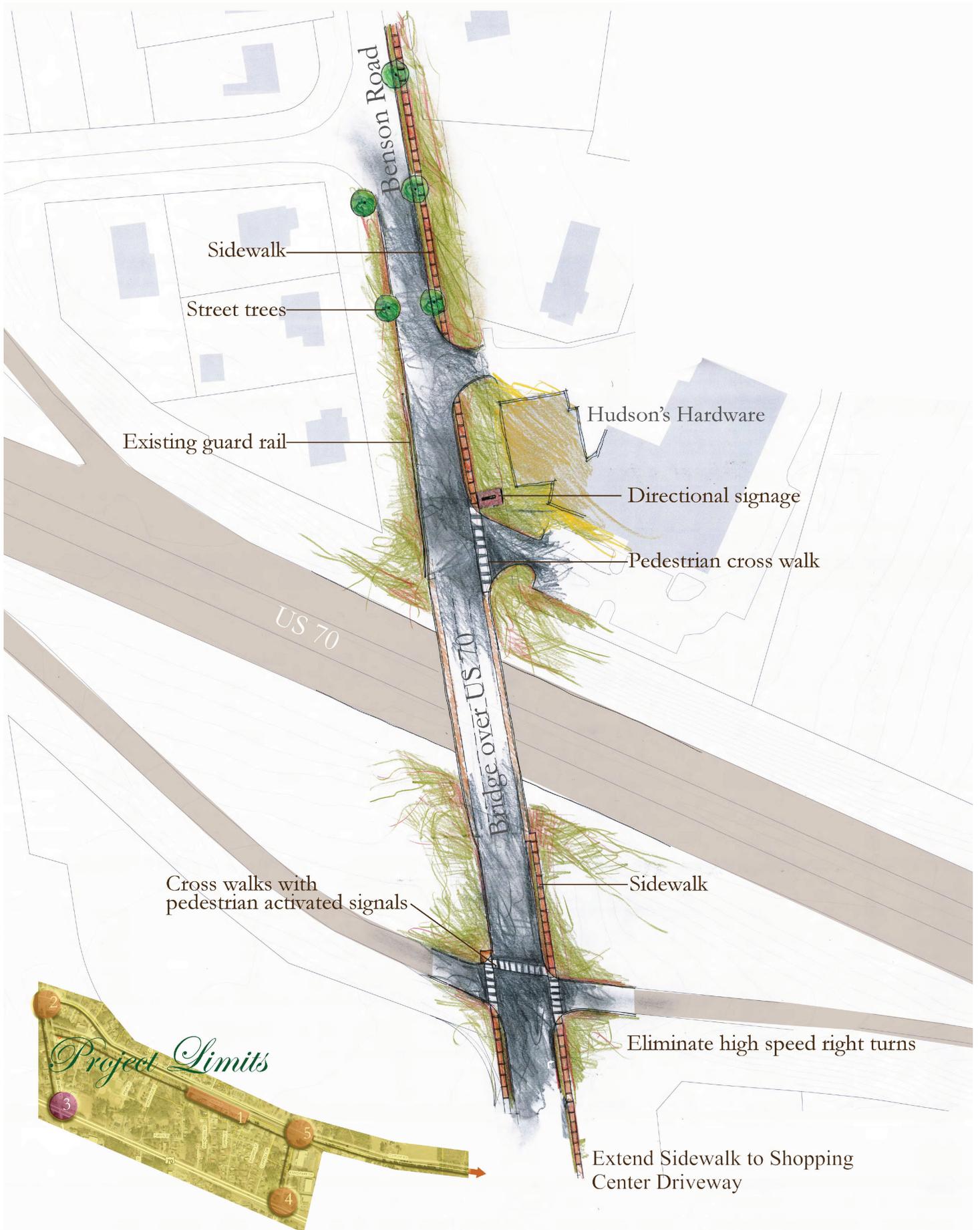
Benson Road traveling towards Main Street



Benson Road traveling towards Main Street



Benson Road bridge over US 70





New Rand at US-70

The following represents the site program for New Rand at US-70 and is an important gateway to Downtown Garner. Several key elements included in the streetscape design are: The addition of deceleration lane with 250 foot taper on both US 70, widen New Rand Road from ten foot to eleven foot lanes, curb and gut-

Site Program

- Add deceleration lane with 250 foot taper on both US 70 and New Rand Road
- Widen New Rand Road from ten foot to eleven foot lanes
- Widen New Rand at US 70 to accommodate turn lane
- Curb and gutter New Rand from US 70 to Main Street
- Add sidewalk system on east side of New Rand Road from US 70 to Main Street
- Add streetscape planting beginning north of turn lane along New Rand Road and ending at the intersection of Main Street
- Add decorative street lighting to match Main Street (NCDOT approval needed)
- Move all utilities underground
- Add crosswalks with pedestrian-activated signals across US 70
- Add Gateway signage

ter New Rand Road, add sidewalk system on east of New Rand Road from US 70 to Main Street, and add decorative street lighting to match Main Street.



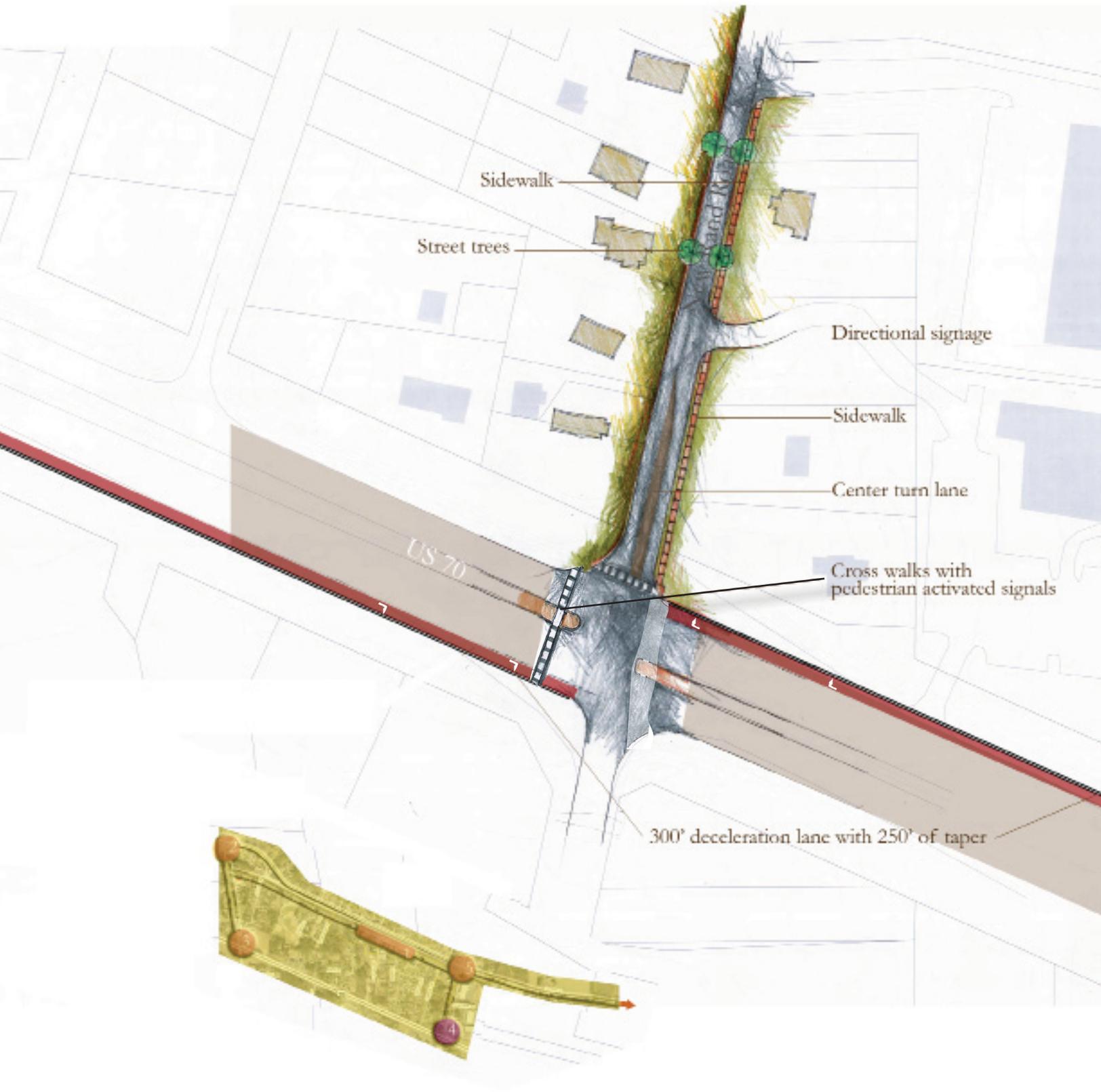
Open swales along New Rand Road



Drainage along New Rand Road



Intersection of US 70 and New Rand Road



Note: It is recommended that a streetscape design concept be developed for New Rand South from US 70 to Rand Mill Road.



New Rand at Garner Road

The following represents the site program for New Rand and Garner Road and is an important linkage from Garner Road to Main Street, Downtown Garner. Several key elements included in the streetscape design are:

Site Program

- Eliminate fast right turns at Garner Road intersection
- Widen New Rand Road from ten foot to eleven foot lanes
- Curb and gutter New Rand from US 70 to Main Street
- Add sidewalk system on east side of New Rand Road from US 70 to Main Street
- Add streetscape planting beginning north of turn lane along New Rand Road and ending at the intersection of Main Street
- Add crosswalks with pedestrian activated signals across Garner Road

The elimination of fast right turns at Garner Road and New Rand Road, adding cross walks with pedestrian activated signals, extending the current sidewalk system on east side along entire corridor, adding directional signage north of bridge, and adding streetscape planting north of the turn lane along New Rand Road.



New Rand Road at NCR
rail crossing



New Rand Road at NCR
rail crossing



Looking back to New
Rand Road from Main
Street



General Design Guidance

Design Guidelines

THE PURPOSE OF THE DESIGN GUIDELINES is to promote future development that is consistent with the Garner Downtown Streetscape Master Plan. Generally, these design guidelines are intended to provide a general outline of preferred design elements in order to insure continuity from this plan to future design efforts. These general guidelines are intended to support local and regional development goals that promote alternative transportation, mix of uses, pedestrian environments, traffic calming, active living, and greenspace. Additionally, providing safe and comfortable environments for the residence and visitors of downtown Garner. These guidelines are intended as a starting point for the development of future detailed guidelines that can be implemented throughout the study area as well as Garner at large.

Street Features

The streets in Garner are the primary community domain and front door for private development. As such, careful consideration of the design elements of the streets are important for enhancing the experiential quality of Downtown Garner as well as communicating appropriate messages about Garner to the citizenry at large.

Street Features

Crosswalks, Bulb-outs, Wayfinding, Lighting, Roundabouts, Sidewalks, Street Furniture, Utilities, Street Trees

Types of public gateways

Town Gateways, District Gateways, Neighborhood Gateways

Gateways

Gateways provide visual interest and denote areas of importance to Downtown Garner. Gateways provide opportunities for directional signage, pedestrian amenities, such as bus shelters, and create focal points for district entrances.

Street Features

Crosswalks, Curbs and Curb Ramps:

Purpose:

Crosswalks, curbs and curb ramps provide design continuity, ensure proper street drainage, and allow pedestrians to safely cross the.

Application:

All crosswalks will be prominently defined and adhere to ADA standards by use of one or more of the following:

- Pavers or textured and painted asphalt that delineate crosswalks.
- Raised crosswalks and traffic tables.
- Electrical crossing signals.
- If the crosswalk is not raised, a curb ramp down to street level must be created in accordance with ADA standards.
- All areas that have a sidewalk must also have a curb.
- Curbs shall allow proper drainage while at the same time delineate the street from adjacent elements such as the sidewalks.
- Curbs shall be no higher than 6”.

Bulb-outs and On Street Parking

Purpose:

Bulb-outs (curb extensions) are encouraged at intersections to reduce pedestrian crossing distance and to provide traffic calming to reduce vehicular speeds.

Application:

- Bulb-outs can be used to separate and define on-street parking areas as well as provide opportunities for landscaping.

Wayfinding:

Purpose:

Way-finding assists pedestrians and automobiles in navigating Downtown Garn and its surrounding region.

Application:

- Signage shall be employed as the primary means of wayfinding
- All street name signs should be located in an



Crosswalk



Bulb-out



Wayfinding

intersection with an intersecting street sign.

Where there are traffic lights, they should be overhead.

- Additional way-finding signage shall be located in the street furniture zone on marquees or posts.
- No way-finding signage shall be in the form of a billboard.
- No way-finding signage shall be illuminated.
- Maps should be utilized to assist in wayfinding.



Wayfinding

Lighting

Purpose:

Proper illumination provides for a safe visibility for vehicles and pedestrians in dim or dark conditions as well as identifying key places within the town. Lighting is a key element in safety and perception of safety for pedestrians. CEPTED (Crime Prevention through Environmental Design) guidelines suggest that proper lighting is a key component of crime prevention. In addition, lighting may highlight special street features such as trees, art, or fountains for an enhanced visual experience.

Application:

- Shall aesthetically complement the design of the street features and surrounding development.
- Shall be at a pedestrian scale as well as service vehicles.
- Should complement the architecture and scale of Garner's Downtown.
- Shall be integrated with all other streetscape elements including trees, benches, bollard lighting, and paving.
- Luminaries shall be translucent or glare-free.
- Light poles shall be a minimum of 15' away from street trees.

Roundabouts

Purpose:

Roundabouts allow for traffic calming without stopping the flow of traffic. Traffic circles also provide a focal element that can direct traffic to town destinations.

Application:

- Roundabouts should naturally cause the traffic to slow without the use of traffic lights.
- Roundabouts should be clearly and simply designed and meet current NCDOT standards.
- Traffic circles may employ paving to delineate it from the rest of the street.
- The center of the roundabout should be free of any vertical elements



Roundabout

Sidewalks:

Purpose:

Sidewalks allow for pedestrian access throughout the study area and to the various uses in the corridor. Sidewalks are places where the public and private interact and transition from one to another through such venues as outdoor dining, bus stops, etc.

Application:

Provide a smooth transition between uses as well as create linkages between uses within the study area. Within the study area sidewalks shall be divided into two zones:

Clear Zone – This zone is adjacent to the face of commercial buildings and shall be between 5' - 15'. The zone shall remain open at all times for pedestrian circulation.

Furniture Zone (or Planting Strip in residential areas) – This zone is adjacent to the curb and shall provide a place for all street furniture.

Street Furniture:

Purpose:

The street furniture zone is the location where the varying public uses such as bicycle parking, seating, trash receptacles, newspaper racks and bollards are located.

Application:

BICYCLE RACKS

- Will be permanently affixed in prominent locations throughout the study area to encourage bicycle use.
- Will be placed so that all bicycles are completely in the street furniture zone.
- Will be well designed and rust proof.
- Minimum of one bicycle space for every twenty automobile spaces.

SEATING, BENCHES

- Should be grouped as much as possible and located along commercial corridors as well as identified pedestrian nodes.
- Planters designed for seating along the edge are permitted.
- Should be well designed to blend in with other surroundings.
- Benches should be free from advertising signage.

TRASH RECEPTACLES

- Placed by every seating area and significant gathering places.
- Shall be permanently affixed.
- Will have a top to keep contents relatively dry.
- Restaurants with outdoor seating shall provide additional trash receptacles near seating.

BOLLARDS

- Designed to complement the streetscape.
- Shall allow parked car doors to be fully opened without obstructing the swing by being set back from the curb.
- May be chained together for the safety of the pedestrian.

NEWSPAPER RACKS

- Should be grouped together in a single location within each area.

Bike Rack



Bench



Trash Recepticals



Bollards



- Shall not compromise the safety of vehicles by obstructing the view.
- Located completely within the street furniture zone.
- Should complement other street furniture elements.

Utilities

Purpose:

Utilities provide for the safe and efficient use of the corridor.

Application:

- All electrical wiring shall be underground.
- Utility boxes, meters, man hole covers and fire hydrants shall coordinate with street furniture.

Street Trees

Purpose:

Street trees provide for the physical and physiological health of the community and environment. Street trees also identify and delineate areas within the corridor

Application:

- Trees shall be planted in the street furniture zone.
- Trees shall be planted as shown.
- Trees shall be a minimum of 15' away from light poles.
- Tree planting pits with grates shall be used in all high pedestrian traffic areas.
- Tree planting pits should have an outlet to allow for special lighting such as string lights or up lighting.
- Tree grates shall be made of ductile iron and be factory painted. They shall allow for tree growth.
- Recommended street trees species include: Dynasty Elm, Zelkova, October Glory Red Maple, and Shumard Oak.



Tree Grate

District Gateways

Purpose:

District Gateways create focal points for district entrances and direct consumers to focus areas within Garner. Gateways provide opportunities for informational and directional signage as well as pedestrian amenities such as bus shelters and seating. District gateway will provide a place to highlight the unique character of the Historic District and Downtown Garner.

Application:

- All district gateways must be approved by the Planning Department as well as NCDOT.
- District gateways may use, but are not limited to, trees, landscaping, art, signage, paving or other elements in their design.
- District gateways will be maintained by the Town of Garner
- If the gateway is accessible, it must comply with ADA.





Examples of Gateways

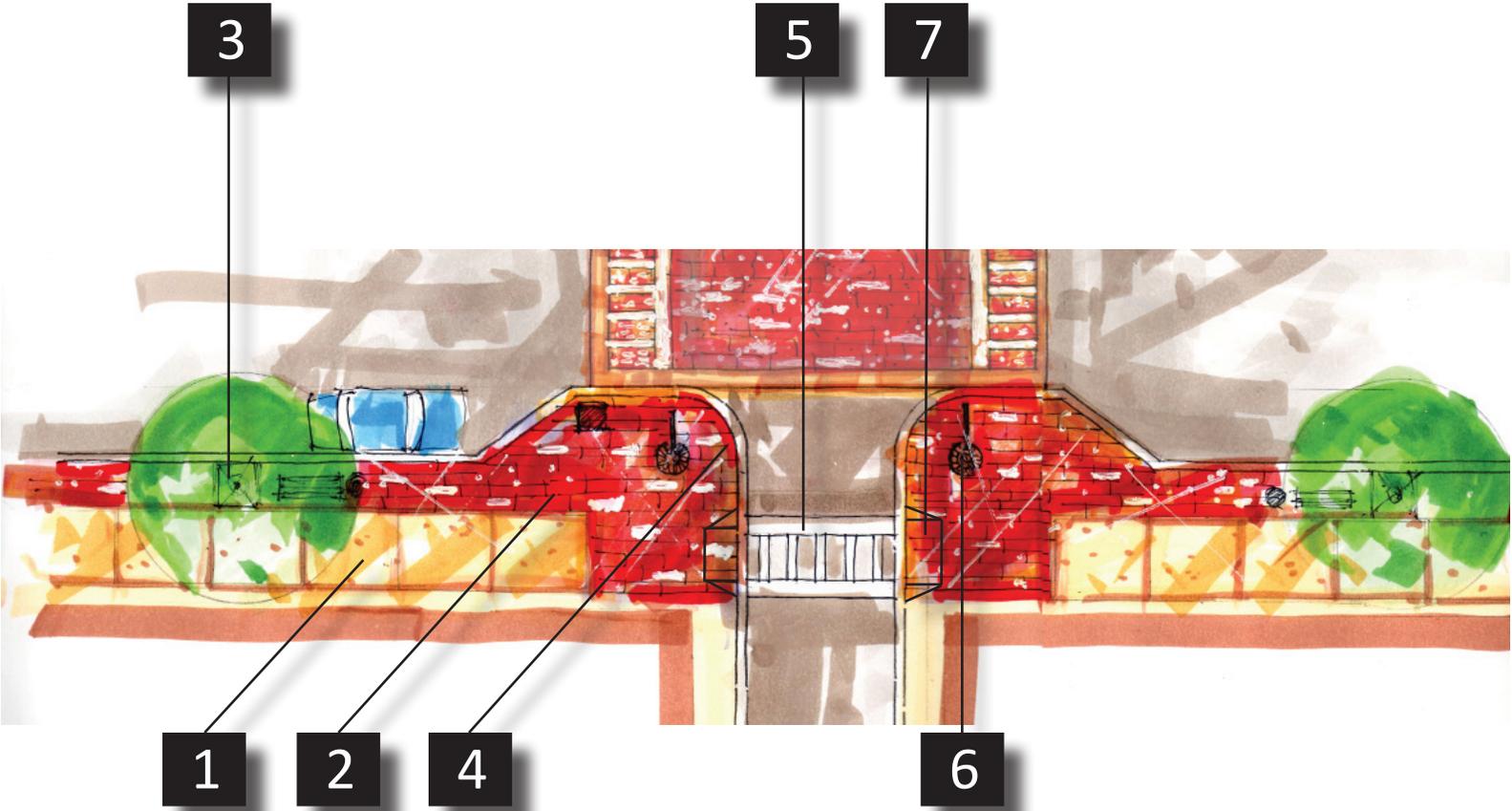


Streetscape Features

The street is the primary public domain and front door for private development. In order to encourage investment and increase pedestrian traffic careful consideration of the features along the street is paramount. Elements and street features should be designed to enhance the visual character and experience of the street.

1. Clear Zone-Public space in front of retail stores that make up the active sidewalk.
2. Furniture Zone-The area between the back of curb and the Clear Zone. Elements within this zone include: street trees, street lighting, bike racks, mail boxes, newspaper stands, benches, trash receptacles, and utilities.
3. Street Trees-Broad canopy trees for shade and streetscape continuity. Should be trimmed to 7' clear height to prevent conflict with retail shops.
4. Bulb-outs- Allow for shorter distances for pedestrians to cross streets while providing traffic calming measures.
5. Crosswalk-Pedestrian crossings at street intersections that have both visual and textural variation.
6. Street lights- Overhead lighting that enhances visibility and safety while providing continuity to the streetscape. Overhead lighting should be interspersed with pedestrian scale lighting.
7. Handicap Ramp- Handicap accessible ramps are located where cross walks meet curbs. ADA compliant materials such as detectable warning pavers should be installed at front of ramp area.

Detail of Streetscape Features



Site Furnishings



Bench

Manchester MC-110 Bench by
BRP Enterprises.
<http://www.brponline.com>

Offered in 48", 72" or 96" length

- Choice of ends/supports: Steel bar, cast iron, or steel pipe ends; Steel bar or steel pipe supports
- Center armrest available
- Optional cast iron end lettering and emblems available
- Custom modular units available
- Manufactured with parts that contain 100% recycled raw materials

Tree Grate

Camelia by Ironsmith.
<http://www.ironsmith.cc>

Camelia resembles a stylized flower. The geometric treatment is an alternative to traditional radial and circular designs.





Bollard

Central Park Series BCP18L by Antique Street Lamps.
<http://www.brponline.com>

The bollard shall be cast iron construction with a classic decorative base. The bollard shall be provided with an optical assembly consisting of a lens and reflector assembly providing an I.E.S. Type V distribution, and a cast aluminum dome top.



Bench

Receptacles AH-403 Receptacle by BRP Enterprises.
<http://www.brponline.com>

- Capacities of 30, 36 and 40 gallon
- Choice of flat, domed, ash or spring top
- Lids secured with vinyl-coated stainless steel aircraft cable
- Rigid plastic liner included
- Optional cast iron emblems available
- Interchangeable with Manchester(TM) collection
- Manufactured with parts that contain 100% recycled raw materials

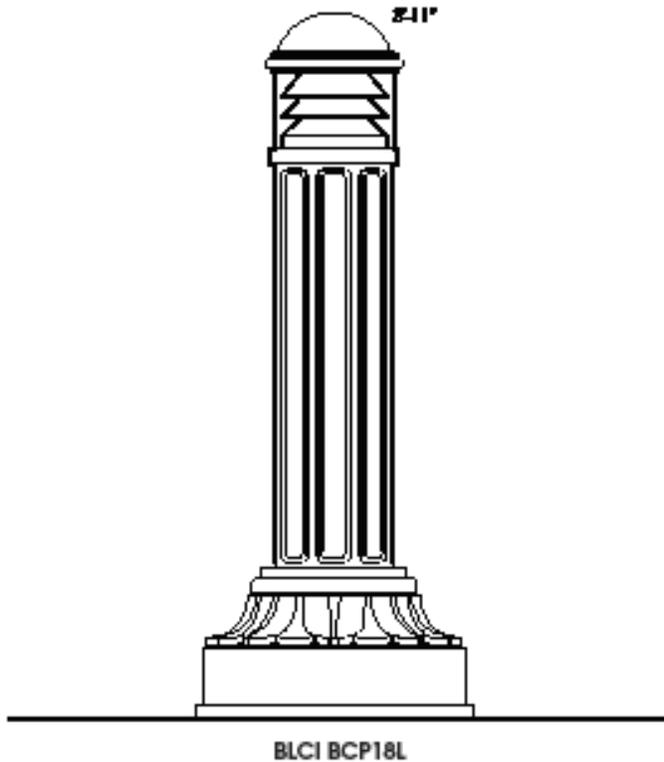


Bike Rack

LL-02 Bike Rack by BRP Enterprises.
<http://www.brponline.com>

- Styles: flip-up, wave, crestline, loopleveline, gridstand, hoop-up or highwheeler
- Capacities ranging from 2 through 14
- Loopleveline bike racks are ideal for those smaller areas where lock up and vandal free maintenance is necessary. Embedded mount and the ability to handle only a few bikes at a time make a distinctive solution for a distinctive environment.
- Manufactured with parts that contain 100% recycled raw materials

CENTRAL PARK Series Cast Iron Lighted Bollard



BLCI BCP18L

SPECIFICATIONS

DESCRIPTION The bollard shall be cast iron construction with a classic decorative base. The bollard shall be provided with an optical assembly consisting of a lens and reflector assembly providing an I.E.S. Type V distribution, and a cast aluminum dome top.

MATERIALS The bollard material shall be ASTM-A48 Class 30 cast iron. The optical assembly top shall be heavy-wall cast aluminum produced from certified ASTM 365.1 ingot per ASTM B179-96a or ASTM B26-96. The castings shall be formed true to the pattern with complete detail. The lens shall be acrylic and the reflector shall be anodized aluminum. All exposed hardware shall be temper resistant stainless steel. Anchor bolts to be completely hot-dip galvanized.

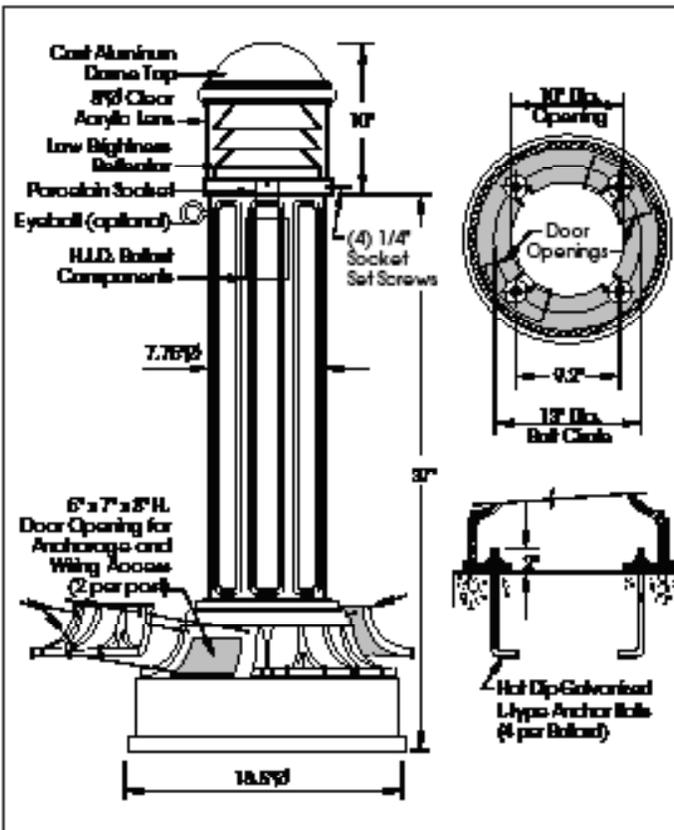
CONSTRUCTION The bollard shall be made from a one-piece casting. The optical assembly shall be attached to the bollard with four temper resistant set screws.

DIMENSIONS The bollard shall be 3'-11" in height with an 18.6" diameter base, a 7.76" diameter shaft and an 8" diameter lens.

INSTALLATION The bollard shall be provided with four L-type anchor bolts to be installed on a 13" diameter bolt circle. Two doors shall be provided in the base for anchorage and wiring access.

LIGHT SOURCE The lighted bollard shall be furnished with an HLD, bollard and socket assembly. Sockets shall be glazed porcelain medium base, with a copper alloy nickel plated screw shell and center contact. The ballast shall be a core and coil, high power factor, regulating type. The bollard shall be UL listed and labeled as suitable for wet locations.

For finish specifications and color options, see "Finish" section in catalog.



ORDERING INFORMATION

Choose the ballast and socket assembly that best suits your needs and select an appropriate lamp.

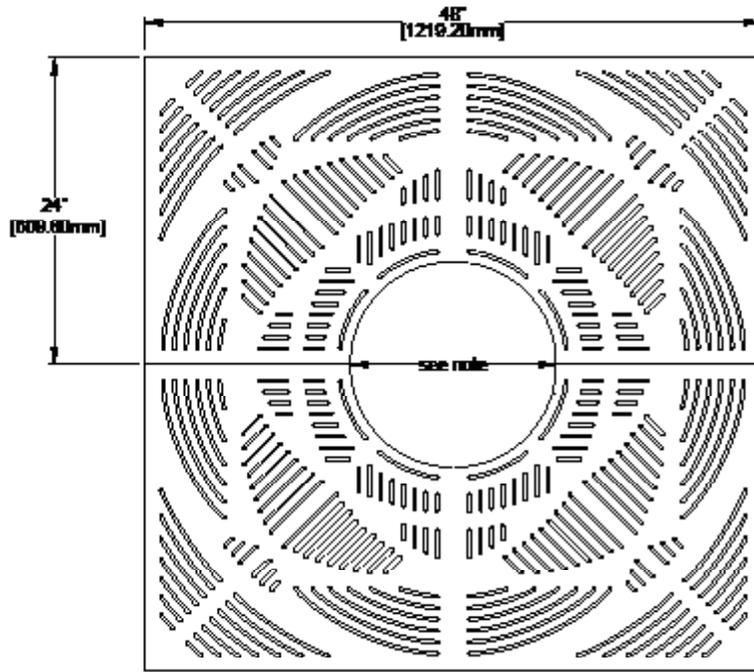
Example: BLCI BCP18L 70M MED ACS 06S 12S P81 ANBK

Bollard	Lens Type	Voltage
BLCI BCP18L	ACS Acrylic, Clear Smooth (Std) ANS Acrylic, White Smooth	120 208 240 277 347 480 575
Voltage/Lamp*		
80M	MED 58-watt Metal Halide	Options
70M	MED 70-watt Metal Halide	
34S	MED 35-watt High Pressure Sodium	
80S	MED 58-watt High Pressure Sodium	
70S	MED 70-watt High Pressure Sodium	
42H7	42-watt compact fluorescent	
Distribution		
06S	Std, Low Lightness Ballast, Type V	Finish†
06S	Class Ballast, Type V	
06S	Class Ballast, Type II	

*Specify Std, Low Lightness Ballast, Type V
06S Class Ballast, Type V
06S Class Ballast, Type II

NOTE:
 1. For finish and color options, see Finish section in catalog.
 2. All lamps are ballast (70000, 30000). For wattage and 70S, 70M or 80M contact us for availability.
 3. For ballast and socket assembly, see the ballast and socket assembly section in catalog.
 4. For other wattage choices, contact us.

ANTIQUE Street Lamps™
 2041-B-42 Durdzog Ln., Austin, TX 78758 • phone 512-459-4097 • fax 512-977-9522



**4834 CAMELIA
TREE GRATE**

48" x 48" tree grate in two sections.

5/16" Maximum slot opening for pedestrian safety and A.D.A Compliance.

Cast from 100% recycled Iron, Aluminum, or Bronze for pedestrian loads only.

Tree opening: 16", 18"
Grates can be ordered with or later expanded to these openings. please specify when ordering.

Finish: unfinished or Black dip or Enamel paint or Polyurethane Paint or Powder coat
Specify finish and color

Use frame models:
4800F

Weight:
Iron= 305 lb/ 139 Kg
Aluminum=110 lb/50 Kg

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IRONSMITH
41-701 Corporate Way #3
Palm Desert, CA 92260
800.338.4788

Project Cost

1. Downtown Core (Main Street):	Quantity	Measure	Cost
Bury all utilities along Main Street from New Rand to Benson Road	4,400	\$175	\$770,000
Replace and add curb and gutter along Main Street	8,800	\$50	\$440,000
Brush sweep and score concrete clear zone	2,000	\$6	\$12,000
Brick furnishing zone	2,000	\$20	\$40,000
Brick bulb-outs	4	\$6,000	\$24,000
Relocate existing lamp posts and add 1' concrete base			\$50,000
lighted bollards	11	\$4,000	\$44,000
Benches	4	\$600	\$2,400
Trash receptacles	4	\$300	\$1,200
Bike racks	2	\$300	\$600
Street trees			\$6,600
Metal tree grates	8	\$200	\$1,600
Brick square / speed table	1,000	\$20	\$20,000
Informational, directional, and cultural signage	3	\$5,000	\$15,000
Crosswalks	5	\$3,000	\$15,000
Restripe existing parking from angular to perpendicular			\$5,000
Plant wild flowers between railroad and Main Street	7,500	\$2	\$15,000
		Sub-total	\$14,62,400
		Cont.	\$270,000
Downtown Core (Main Street)		Total	\$1,732,400
2. Benson Road at West Garner and Main Street:	Quantity	Measure	Cost
Replace and add curb and gutter along Benson Road as needed	400	\$50	\$20,000
Add concrete sidewalks along east side of Benson Road	400	\$4	\$1,600
Add chokers and bulb-outs	2	\$3,000	\$6,000
Install pedestrian crosswalks and activated signals at all corners	4	\$5,000	\$20,000
Install gateway and directional signage	2	\$3,000	\$6,000
Replace signals with black mast arm poles (includes pre-drilled piers)	4	\$15,000	\$60,000
Add retaining wall under NC Railroad bridge to facilitate sidewalk	50	\$500	\$25,000
Street trees and streetscape planting			\$10,000
Install pedestrian crosswalks at intersection	4	\$1,000	\$4,000
Add four lamp posts on each side of intersection	4	\$7,500	\$30,000
		Sub-total	\$322,600
		Cont.	\$35,000
Benson Road at West Garner and Main Street		Total	\$357,600

3. Benson Road at US-70 to Main Street:	Quantity	Measure	Cost
Replace and add curb and gutter along Benson Road as needed	2,200	\$50	\$110,000
Add concrete sidewalks south of bridge	100	\$4	\$400
Add concrete sidewalks along east side of Benson Road to Main Street	1,100	\$4	\$4,400
Add chokers	2	\$3,000	\$6,000
Install pedestrian crosswalks and activated signals at all corners	1	\$5,000	\$5,000
Install pedestrian crosswalks	3	\$3,000	\$9,000
Install gateway and directional signage north of bridge	1	\$3,000	\$3,000
Street trees and streetscape planting			\$3,000
		Sub-total	\$140,800
		Cont.	\$35,000
Benson Road at US-70 to Main Street		Total	\$175,800
4. New Rand Road at US-70:	Quantity	Measure	Cost
Install curb and gutter and widen New Rand Road from US 70 to Main Street	2,400	\$50	\$120,000
Add concrete sidewalks along east side of New Rand Road to Main Street	1,200	\$4	\$4,800
Bury all utilities	1,200	\$175	\$210,000
Install pedestrian crosswalks and activated signals at US 70	1	\$5,000	\$5,000
Install pedestrian crosswalks	1	\$3,000	\$3,000
Install gateway and directional signage	1	\$3,000	\$3,000
Street trees and streetscape planting			\$3,000
Add historic lamp posts along both sides of New Rand Road to Main Street	30	\$7,500	\$225,000
		Sub-total	\$573,800
		Cont.	\$35,000
New Rand Road at US-70		Total	\$608,800
5. New Rand Road and West Garner Road:	Quantity	Measure	Cost
Install pedestrian cross walks and activated signals at Garner	1	\$10,000	\$10,000
Install pedestrian crosswalks	2	\$3,000	\$6,000
Install gateway and directional signage	1	\$3,000	\$3,000
Street trees and streetscape planting			\$3,000
		Sub-total	\$22,000
		Cont.	\$2,200
New Rand Road and West Garner Road		Total	\$24,200

Fast-Track Projects

The projects at right, ordered in five main tiers, describe the top priorities based on constructability and impact towards achieving the goals and objectives of the Streetscape Design Plan.

Green: Main Street Area

Blue: Benson Road/Main Street Intersection

Brown: Benson Road

Rose: New Rand Road and US 70 Intersection

Critical Sidewalk Elements

Establishing a strong pedestrian linkage throughout the study area is important; the following sidewalks are priorities for the implementation of the Streetscape Master Plan:

1. Main Street from Pearl St. to Benson Rd.
2. East side of Benson Rd. to W. Garner Rd.
3. East side of Benson Rd. from Main Street to US Highway 70.

New Rand Road will also have sidewalks created as part of an overall corridor improvement for that facility.

Fast-Track Projects	Quantity	Measure	Cost
Relocate existing lamp posts and add 1' concrete base along Main Street			\$50,000
Brick square / speed table at Main Street	1,000	\$20	\$20,000
Plant wildflowers between railroad and Main Street	7,500	\$2	\$15,000
Install pedestrian crosswalks	4	\$3,000	\$4,000
Add four lamp posts on each side of intersection	4	\$7,500	\$30,000
Replace signals with black mast arm signal poles (incl. pre-drilled piers)	4	\$15,000	\$60,000
Replace and add curb and gutter along Benson Road as needed	400	\$50	\$20,000
Add concrete sidewalks along east side of Benson Road	400	\$4	\$1,600
Add chokers and bulb-outs at Benson Rd and Main Street	2	\$3,000	\$6,000
Install pedestrian cross walks and activated signals at all corners	4	\$5,000	\$20,000
Install gateway and directional signage at Benson Road and West Garner	2	\$3,000	\$6,000
Add retaining wall under NC Railroad bridge to facilitate sidewalk	50	\$500	\$25,000
Street trees and streetscape planting along Benson Road			\$10,000
Add concrete sidewalks along east side of Benson Road	400	\$4	\$1,600
Add concrete sidewalks along east side of Benson Road to Main Street	1,100	\$4	\$4,400
Add concrete sidewalks along east side of New Rand Road to Main Street	1,200	\$4	\$4,800
Install pedestrian crosswalks and activated signals at US 70/New Rand Rd	1	\$5,000	\$5,000
Install pedestrian crosswalks at US 70/New Rand Road	1	\$3,000	\$3,000
Install gateway and directional signage at US 70 and New Rand Road	1	\$3,000	\$3,000
Overall Fast Tract Recommendations + Contingency		Total	\$336,340

